



MEMORANDUM

Date 1 December 2011

To RTPC Managers

From Brad Beck

RE Applications Received for Measure J CC-TLC Program

In response to the call for projects for the Measure J Transportation for Livable Communities (CC-TLC) program, the Authority received 23 applications requesting, in total, \$16.6 million. Overall, the amount requested exceeds the amount available through the CC-TLC program by about \$3 million. The following table summarizes the funding requests and amounts available by RTPC. (As you know, no further CC-TLC funds are available to East County.)

<i>RTPC</i>	<i>Applications Received</i>	<i>Requested Funding</i>	<i>Amount Available</i>	<i>Difference</i>
WCCTAC ¹	5	\$6,625,580	\$6,167,943	-\$457,637
TRANSPAC	10	\$6,277,200	\$5,166,881 ²	-\$1,110,319
SWAT	8	\$3,964,646	\$3,582,445	-\$382,201
Total ³	23	\$16,672,426	\$14,917,269	-\$1,755,157

- 1 The amount available to WCCTAC includes the additional \$1,379,130 available through Program 25b, Additional Transportation for Livable Communities.
- 2 The amount available was reduced to account for the previous allocation of \$600,000 to projects in Pleasant Hill and Martinez.
- 3 Total CC-TLC funds requested does not equal total for the three subareas because the Olympic Boulevard Trail Connector Study is included in both the TRANSPAC and SWAT subareas.

Schedule

The schedule for RTPC review of the CC-TLC applications has review by the technical advisory committees occurring in December, 2011 and January, 2012 with RTPC board

approval in February. The Authority would approve the funding recommendations in April, 2012.

Programming the Projects

The estimates of the funding available through the CC-TLC program come from the 2011 Measure J Strategic Plan. The Measure J Strategic Plan estimates how much revenue the Authority is expected to receive each year taking into consideration both increases in sales and inflation. From staff's cursory review of the applications received, it appears that a large share of the funding would be requested in the early years of the program. Unfortunately, those requests will likely exceed the available cash flow for the program.

For that reason, we are asking that the RTPC establish priorities among the projects recommended to the Authority. The final table below shows how the estimated CC-TLC funds would be allocated among the subareas by fiscal year. It subtracts out the CC-TLC funding that the RTPCs have already allocated.

Each RTPC's program of projects need not precisely match the funding constraints outlined in Table 2 below. For the initial allocation of CC-TLC funds, a list of recommended projects in priority order would be sufficient. Authority staff would then program the projects within the estimated funding available in that fiscal year.

The RTPCs could use several reasons to designate a project as higher priority:

1. **Funding Deadlines:** The project has other funding that must be expending within the near term
2. **Project Readiness:** The project has design, right-of-way and environmental clearance completed
3. **Pressing Need:** The project addresses a pressing issue, for example, an improvement needed to support a development project or needed to remedy an dangerous condition for pedestrians or bicyclists

OLYMPIC CORRIDOR TRAIL CONNECTOR STUDY

All but one of the projects submitted are located entirely within one subarea. The one exception is the Olympic Corridor Trail Connector Study which is located in both the SWAT and TRANSPAC areas. The sponsor of the project, Contra Costa County, is requesting \$195,000 to identify a preferred alignment for a pedestrian/bicycle connection between Lafayette and Walnut Creek along Olympic Boulevard.

In the table at the beginning of this memo, we have included this project in the totals for both SWAT and TRANSPAC areas. Since this study would need the support, coop-

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eration and involvement of both Lafayette and Walnut Creek, as well as both RTPCs, Authority assumes that SWAT and TRANSPAC would need to determine a cost sharing arrangement.

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Table 1 — Applications Received

<i>Project Name</i>	<i>Project Location</i>	<i>Sponsor</i>	<i>12 - CC-TLC</i>	<i>13 - PBTF</i>	<i>Other Funds</i>	<i>Unfunded Balance</i>	<i>Total Project Cost</i>
Transportation for Livable Communities							
SWAT							
Livable Moraga Road	Moraga Road, Rheem Park Planning Area	Moraga	\$325,000		\$50,000		\$375,000
Iron Horse Trail Corridor Improvements	Iron Horse Trail between Bollinger Canyon Rd and San Ramon/Dublin border	San Ramon	\$330,950		\$367,045		\$697,995
Orinda-Lafayette BART Wayfinding & Lighting Improvement Project *	Orinda & Lafayette BART Stations, City of Orinda	BART	\$394,100		\$200,000		\$594,100
East End Ped/Bike & Streetscape Improvements, Phase 1	Mt. Diablo Boulevard, First Street to Brown Avenue	Lafayette	\$495,000		\$40,000		\$535,000
San Ramon Valley Iron Horse Trail Bicycle/Pedestrian Overcrossings	Iron Horse Trail at Bollinger Canyon Rd and Iron Horse Trail at Crow Canyon Rd.	San Ramon	\$558,000		\$0	\$10,984,000	\$11,542,000
Downtown Danville Multimodal Access Improvements	Downtown Danville, along Hartz and Railroad Avenues north of San Ramon Valley Boulevard and south of Danville Boulevard	Danville	\$876,596		\$660,800		\$1,537,396
Stone Valley Road Bike Lane Gap Closure	Along Stone Valley Road from High Eagle Road to Winding Glen	Contra Costa County	\$700,000		\$400,000		\$1,100,000

Project Name	Project Location	Sponsor	12 - CC-TLC	13 - PBTF	Other Funds	Unfunded Balance	Total Project Cost
Olympic Corridor Trail Connector Study **	Olympic Blvd corridor from the intersection of the Lafayette-Moraga Trail, Olympic Blvd, & Reliez Station Road in the City of Lafayette to the Iron Horse Trail Corridor in the City of Walnut Creek	Contra Costa County	\$195,000		0		\$195,000
Total for SWAT			\$3,964,646		\$1,717,845	\$10,984,000	\$16,666,491
TRANSPAC							
Adaptive Service Analysis Plan	Cities of Clayton, Walnut Creek, Concord, Pleasant Hill, and unincorporated portions of Contra Costa	Other	\$90,000		\$25,000		\$115,000
Contra Costa Centre Treat Blvd/I680 Bicycle and Pedestrian Plan	Unincorporated Contra Costa/City of Walnut Creek	Contra Costa County	\$100,000		0		\$100,000
Walnut Creek Pedestrian Master Plan	Citywide	Walnut Creek	\$100,000		\$20,000		\$120,000
Northwest Walnut Creek Pedestrian and Bicycle Access	Buena Vista Avenue (Geary Rd-Parkside Dr); Parkside Drive (Buena Vista Ave-Hillside Dr); Hillside Drive (Parkside Dr-East end of 680 overpass connecting to existing sidewalk to BART); San Juan Avenue (Alvarado Ave to south end of Elementary school property line)	Walnut Creek	\$1,000,000		\$250,000		\$1,250,000

<i>Project Name</i>	<i>Project Location</i>	<i>Sponsor</i>	<i>12 - CC-TLC</i>	<i>13 - PBTF</i>	<i>Other Funds</i>	<i>Unfunded Balance</i>	<i>Total Project Cost</i>
Olympic Corridor Trail Connector Study **	Olympic Blvd corridor from the intersection of the Lafayette-Moraga Trail, Olympic Blvd, & Reliez Station Road in the City of Lafayette to the Iron Horse Trail Corridor in the City of Walnut Creek	Contra Costa County	\$195,000		—		\$195,000
Pacheco Blvd Bike and Pedestrian Project	North side of Pacheco Blvd, Pacheco; from Windhover Way to 230ft south of Morello Avenue	Contra Costa County	\$850,000		\$300,000		\$1,150,000
Central Concord Pedestrian Improvements & Streetscape Project	Various Streets in Central Concord including Clayton Rd, Willow Pass Rd between Market St., and Galindo Street and the downtown area including Grant St., and Salvio St.	Concord	\$3,000,000		\$535,000		\$3,535,000
Central Contra Costa Trail Crossing Safety Improvement Study	Within TRANSPAC subregion of Contra Costa County	Contra Costa County and East Bay Regional Park District	\$50,000		—		\$50,000
Citywide Trail Crossing Enhancement Project	Six trail locations along the Contra Costa Canal Trail (Chilpancingo Parkway, Viking Drive, Gregory Lane, Boyd Road, Oak Park Boulevard, and Putnam Boulevard) and two locations along the EBMUD Trail (Oak Park Boulevard and Astrid Drive)	Pleasant Hill	\$371,200		\$914,800		\$1,286,000

Project Name	Project Location	Sponsor	12 - CC-TLC	13 - PBTF	Other Funds	Unfunded Balance	Total Project Cost
Golf Club Road Bridge Widening/Replacement Project	Golf Club Road between Old Quarry Road and Contra Costa Boulevard	Pleasant Hill	\$521,000		\$2,835,000		\$3,356,000
Total for TRANSPAC			\$6,277,200		\$4,879,800	\$0	\$11,157,000
WCCTAC							
Richmond Transit Village Phase II East Side Improvements: Nevin BART to 19th	Nevin Ave., between Richmond BART and 19th Street	Richmond	\$2,960,000		\$1,320,000		\$4,280,000
San Pablo Ave Corridor Complete Streets Plan	San Pablo Avenue including Plaza & Del Norte BART Station in El Cerrito	El Cerrito	\$137,000		\$13,000		\$150,000
Richmond-Ohlone Greenway Gap Closure *	Between I-80 & San Pablo Ave, just north & under BART tracks, & adjacent to Baxter Creek	Richmond	\$888,580		\$765,129		\$1,653,709
San Pablo Dam Road Walkability and Community Enhancement	Downtown El Sobrante	Contra Costa County	\$1,400,000		\$1,027,000		\$2,427,000
Bay Trail Gap Closure Project	City of Hercules	Hercules	\$1,240,000		\$2,954,050		\$4,194,050
Total for WCCTAC			\$6,625,580		\$6,079,179	\$0	\$12,704,759

* Applied for both CC-TLC and PBTF funding

** Project is in both the SWAT and TRANSPAC subareas

Table 2 — Allocation of CC-TLC Funds by Fiscal Year (\$1,000s)

<i>Available Funding</i>	<i>FY09</i>	<i>FY10</i>	<i>FY11</i>	<i>FY12</i>	<i>FY13</i>	<i>FY14</i>	<i>FY15</i>
TLC - 3.5586%	\$0.0	\$0.0	\$4,904.2	\$2,257.9	\$2,326.1	\$2,396.4	\$2,468.7
Less admin	\$0.0	\$0.0	\$4,830.7	\$2,224.1	\$2,291.2	\$2,360.4	\$2,431.7
Additional CC-TLC for West County	\$0.0	\$0.0	\$551.2	\$253.8	\$261.5	\$269.4	\$277.5
Less admin	\$0.0	\$0.0	\$542.9	\$250.0	\$257.5	\$265.3	\$273.3
<i>Share of Funding by Region</i>	<i>FY09</i>	<i>FY10</i>	<i>FY11</i>	<i>FY12</i>	<i>FY13</i>	<i>FY14</i>	<i>FY15</i>
West (33.9%) †	\$0.0	\$0.0	\$2,179.2	\$1,003.3	\$1,033.6	\$1,064.8	\$1,097.0
Central (40.8%)	\$0.0	\$0.0	\$1,970.4	\$907.2	\$934.6	\$962.8	\$991.9
Southwest (25.3%)	\$0.0	\$0.0	\$1,224.0	\$563.6	\$580.6	\$598.1	\$616.2
Total	\$0.0	\$0.0	\$5,373.6	\$2,474.1	\$2,548.8	\$2,625.8	\$2,705.1
<i>Funding Already Allocated</i>	<i>FY09</i>	<i>FY10</i>	<i>FY11</i>	<i>FY12</i>	<i>FY13</i>	<i>FY14</i>	<i>FY15</i>
West	\$0.0	\$0.0	\$0.0	\$210.0	\$0.0	\$0.0	\$0.0
Central	\$0.0	\$0.0	\$0.0	\$600.0	\$0.0	\$0.0	\$0.0
Southwest	\$0.0	\$0.0	\$0.0	\$0.0	\$0.0	\$0.0	\$0.0
	\$0.0	\$0.0	\$0.0	\$810.0	\$0.0	\$0.0	\$0.0
<i>Cumulative Funding Available</i>	<i>FY09</i>	<i>FY10</i>	<i>FY11</i>	<i>FY12</i>	<i>FY13</i>	<i>FY14</i>	<i>FY15</i>
West	\$0.0	\$0.0	\$2,179.2	\$2,972.5	\$4,006.1	\$5,070.9	\$6,167.9
Central	\$0.0	\$0.0	\$1,970.4	\$2,277.6	\$3,212.2	\$4,175.0	\$5,166.9
Southwest	\$0.0	\$0.0	\$1,224.0	\$1,787.6	\$2,368.2	\$2,966.3	\$3,582.4
	\$0.0	\$0.0	\$5,373.6	\$7,037.7	\$9,586.5	\$12,212.2	\$14,917.3

† Includes funds from Program 25b, Additional Transportation for Livable Communities



Application Form

Measure J Transportation for Livable Communities (CC-TLC) Program Funds

1. PROJECT INFORMATION

Project Name Adaptive Service Analysis Plan

Project Location Cities of Clayton, Walnut Creek, Concord, Pleasant Hill, and unincorporated portions of Contra Costa County

Sponsor Central Contra Costa Transit Authority

Implementing agency {if different than sponsor}

Partner agencies {list only those agencies whose role is essential in implementing the proposed project}

Contact for project... Laramie Bowron
 Manager of Planning
 925.680.2048
bowron@cccta.org
 2477 Arnold Industrial Way, Concord, CA 94520

2. FUNDING

In thousands of dollars, please provide the estimated cost of the proposed project, the amount of CC-TLC funds requested, and any other funds that have been committed to the project.

<i>Source</i>	<i>Amount</i>
Total project cost †	\$115
Requested CC-TLC funds	\$90
Other committed funding 1 ‡	\$25
Other committed funding 2 ‡	\$0
Unfunded balance	\$ 230

† Minimum request is \$50,000

‡ Add other committed funding sources as needed

3. POTENTIAL PHASING

Phasing this project would likely result in an insufficient project as both the on-board survey and operating policy alternatives analysis components are needed to effectively achieve the desired results.

4. PROJECT DESCRIPTION

The Central Contra Costa Transit Authority (CCCTA) is seeking CC-TLC funding to undertake an Adaptive Service Analysis Plan. This project shall strive to achieve the goals of improving ridership as well as cost efficiency by analyzing alternative operating policies, such as flex-route service, in the Central Contra Costa region of CCCTA's service area, specifically the Cities of Walnut Creek, Concord, Martinez, and Pleasant Hill as well as unincorporated parts of the County. This Adaptive Service Analysis Plan will include an on-board survey and will cover all levels of service from system management to service type while working to improve integration with the existing regional transportation network. Results of this analysis shall include recommendations and an implementation plan on providing more efficient service that responds to the needs of the community and increases the viability of transit use leading to more livable communities.

This Adaptive Service Analysis Plan seeks to address the lack of productivity in suburban neighborhoods and to better integrate CCCTA's service into the existing regional transportation network. The project aims to achieve this by updating CCCTA's operating policy to one that promotes more productivity, cost efficiency, and one that in results more livable and sustainable communities. Examples of operating policies that CCCTA seeks to explore include: resource redeployment, reducing fleet capacity, implementing demand-responsive connector service, flexible-route segments, point deviation, route deviation, and zone routes, among others. These types of services have produced dramatically successful results in similar suburban neighborhoods resulting in increased walkability, fewer harmful emissions from transportation, higher transit ridership, and impressive cost-savings.

CCCTA's operating policy has been to provide mutually exclusive traditional fixed-route and ADA demand-response services. While this has been a consistent option for passengers it may not be meeting the needs of a growing diverse population. This project, through service analysis and on-board surveys, seeks to study the implementation of alternative operating policies that respond to the diverse needs of the population. The use of flexible transit services has become a more realistic alternative to traditional fixed-route service as growth patterns, economic trends, and social changes typical of suburban development have not favored this type of service. CCCTA's service area is diverse and this diversity includes differences in markets served as well as differences in the physical environment. To have the most success CCCTA needs to creatively adapt to complement this diversity in a way that benefits current and future riders as well as support community and business development. Exploring these alternatives will make available future funding for specialized service and improved coordination between CCCTA, local businesses, and community leaders.

The overall goal of this project is to responsibly enhance existing service as well plan to meet the growing mobility needs of Central Contra Costa County. Project staff shall work with low-income populations, persons with disabilities, as well as members of the general public to achieve the goals of the project. Staff shall also coordinate with regional agencies, relevant City and County jurisdictional representatives as well as students, commuters, etc. to maximize the effectiveness of CCCTA's service.

5. PROPOSED SCHEDULE

In the following table, enter the expected beginning and ending *month and year* for each of the phases listed. If the phase has been finished, enter "Completed" in the "To" column.

<i>Phase</i>	<i>From</i>	<i>To</i>
Project Contracting	05/12	07/12
Create/Administer On-Board Survey Instrument	07/12	11/12
Community/Jurisdictional Input on Findings	11/12	03/13
Adoption by CCCTA Board of Directors	04/13	05/13
Implementation of Recommendations	05/13	11/13

6. ABILITY TO MEET CRITERIA

Describe how the proposed project would meet the following four criteria:

Achievement of CC-TLC Goals:

Identify which of the six goals of the CC-TLC program the proposed project would help achieve — a project need not meet all of them — and *briefly* describe how it would do so (see Exhibit One for these six goals)

1. Help create walkable, pedestrian-friendly neighborhoods and business districts

Transit riders inherently are more walkable than their auto-dependent counterparts as their destinations are generally further from bus or rail stops than parking garages. Therefore, by creating a more successful transit system that increases the number of individuals utilizing it, people will be walking more. By having fewer cars on the road and in the parking lots of business districts, the walkability of that area will increase. Also, by improving the transit system individuals will be more likely to walk/bike to transit rather than drive to it. Land-use and density have a lot to do with walkability but a successful transit system that attracts riders away from automobile use by attending more specifically to their mobility needs will have a more impressive effect on the walkability of neighborhoods and business districts within central Contra Costa County.

2. Promote innovative solutions, including compact building design and context-sensitive site planning that is integrated with the transportation system

This project promotes innovative solutions by analyzing alternative operating policies that adapt to the specific needs of the communities in Central Contra Costa County. Traditional fixed-route transit service, while consistent, may not be appropriate for CCCTA's diverse service area as observed in areas with especially

low productivity. Suburban transit operates in a nontraditional landscape and has attempted to overlay operating techniques that were designed for urban areas with higher density. Solving the problem of low productivity in these areas will require nontraditional, innovative solutions that seek to combine successful practices achieved elsewhere in the country. As technology has expanded and become more widely available, more doors have opened that utilize technology to operate more efficient community-tailored service rather than pre-prescribed regional service. CCCTA seeks to benefit from the technological innovations to implement service that has proven itself innovative yet dependable in similar suburban areas.

3. Help create walkable, pedestrian-friendly access linking housing and job centers to transit.

This project not only seeks to improve the efficiency of the existing transit system but also, through the on-board survey, to identify service gaps that are inhibiting individuals from utilizing the transit system. The recommendations included in the plan will identify solutions for closing those gaps and increasing the productivity of the system. Having identified service gaps, CCCTA will work with the jurisdictions within the central part of CCCTA's service area to obtain future funding and improve coordination to increase the accessibility within the study area.

4. Help create affordable housing

Affordable housing is most successful when developed in close proximity to reliable

transit services. This project will result in a plan to achieve the most efficient reliable transit system possible throughout the study area. This will have dramatic benefits by effectively increasing the area that could adequately support affordable housing.

5. Encourage a mixture of land uses and support a community's development or redevelopment activities

This Adaptive Service Analysis Plan does support communities' development and redevelopment activities by providing a plan that identifies service gaps and barriers that inhibit transit usage through an on-board survey. This survey will provide effective corroboration for future projects that aim to increase transit usage and reduce the auto-dependency within the study area. A significant part of the plan will include working with jurisdictional representatives to ensure that projects and recommendations are coordinated.

6. Provide for a variety of transportation choices to enhance a community's mobility, identity, and quality of life.

This plan will result in enhanced community mobility options by opening up CCCTA's service to those whose mobility needs are not being met by the current fixed-route system. Enhanced walkability, cleaner air, and increased transit opportunities are fundamentally integral to an area's quality of life and this project opens the door to implement a transit service that improves those indicators. Increasing community interaction by discouraging private automobile usage from those in central Contra Costa County will alter the identity of the communities

within its study area.

Feasibility: Describe why the project is feasible and any outstanding issues in the project development process — design, environmental clearance, right-of-way purchase, and PS&E — or funding of the project

This project has no outstanding issues and would be fully funded with the award of CC-TLC funds. Similar projects have been completed for transit agencies of all sizes by consultants around the country. Qualified consultants will have the technical expertise as well as comprehensive knowledge of the array of service options facing CCCTA. Additionally, CCCTA staff has the technical capacity to carry out the project. CCCTA is currently in the process of bidding out two planning projects both of which involve the use of federal, local, and state funds. CCCTA project management staff has over 60 years of combined experience in managing large-scale planning grants. CCCTA has the technical capacity to implement this project and have overseen similar projects that have had successful outcomes.

Local and policy support: Identify local policies that support the project and the integration of the project with other local efforts, as well as other support from the general public, the RTPCs and other relevant agencies

This Adaptive Service Analysis Plan is supported by the Contra Costa Transportation Authority's Central County Action Plan for Routes of Regional Significance. Specifically, goals 5 and 6 that seek to "Support an efficient and effective transit system" and decrease single-occupant vehicle use in Central County.

Also, attached are selections from the General Plans from the Cities of Walnut Creek, Pleasant Hill, and Concord. All of the attached selections illustrate support for the goals of this Adaptive Service Analysis Plan. By improving efficiency and connectivity, encouraging public transit usage, and supporting a reliable alternative to the private automobile, CCCTA seeks to achieve goals established in the attached General Plans.

Matching funds: Identify any funds from other sources that are or would be committed to the project

If awarded CC-TLC funds for this project CCCTA would commit nearly 22% of the total project cost with previously allocated Transportation Development Act (TDA) funds.

EXHIBIT ONE

Goals of the Measure J CC–TLC Program

Measure J establishes six goals for the Measure J Transportation for Livable Communities program.

- Help create walkable, pedestrian-friendly neighborhoods and business districts
- Promote innovative solutions, including compact building design and context-sensitive site planning that is integrated with the transportation system
- Help create walkable, pedestrian-friendly access linking housing and job centers to transit
- Help create affordable housing
- Encourage a mixture of land uses and support a community's development or redevelopment activities
- Provide for a variety of transportation choices to enhance a community's mobility, identity, and quality of life

Adaptive Service Analysis Plan Study Area



**CENTRAL COUNTY
ACTION PLAN**

for

Routes of Regional Significance

Adopted July 9, 2009

GOAL 1	Encourage land use decisions that manage the increase of overall traffic demand
ACTIONS	1-A: Continue to support implementation of the Measure C/J Growth Management Program.
	1-B: Continue to support higher-density development around transit hubs and downtowns.
	1-C: Continue to require each jurisdiction to: <ul style="list-style-type: none"> a) Notice the initiation of the environmental review process for projects generating more than 100 net-new peak-hour vehicle trips. b) For projects that require a General Plan Amendment, identify any conflicts with Action Plan MTSOs and then, if requested, present the analysis results and possible mitigation strategies to TRANSPAC for review and comment.
	1-D: Include the needs of pedestrians and bicyclists in the design, construction, and maintenance of development projects.
	1-E: Continue to implement the TRANSPAC Subregional Transportation Mitigation Program.
RESPONSIBLE AGENCIES	TRANSPAC and its jurisdictions
TIMELINE	These actions are ongoing.

GOAL 2	Increase HOV lane usage
ACTIONS	2-A: Support the completion of a continuous HOV system on I-680.
	2-B: Support consistent occupancy requirements for toll-free HOV lanes on the Benicia-Martinez Bridge and I-680.
	2-C: Support additional incentives for HOV users.
	2-D: Provide additional park-and-ride lots.
RESPONSIBLE AGENCIES	TRANSPAC will continue to advocate for funding and phasing to complete the HOV lane system and to encourage incentives.
TIMELINE	Depending on funding availability, Action 2-A in the southbound direction is intended to be completed by 2014. Other actions are ongoing.

GOAL 3	Work to improve freeway flow
ACTIONS	3-A: Continue to monitor and evaluate operational improvements at freeway interchanges on I-680, SR-242, SR-24, and SR-4.
	3-B: Continue to support the completion of the fourth bore of the Caldecott Tunnel (SR-24).
	3-C: Support the study and implementation of potential regional freeway management strategies.
	3-D: Consider a multi-agency approach to freeway ramp metering.
RESPONSIBLE AGENCIES	TRANSPAC and its jurisdictions
TIMELINE	These actions are ongoing. Depending on funding availability, target completion of the Caldecott Tunnel fourth bore is 2014.

GOAL 4	Manage arterial traffic flow
ACTIONS	4-A: Seek funding for traffic and transit improvements along Regional Routes.
	4-B: Continue to implement the Central Contra Costa Traffic Management Program.
	4-C: Where feasible and appropriate, address the needs of pedestrians and bicyclists along Regional Routes.
RESPONSIBLE AGENCIES	TRANSPAC and its jurisdictions
TIMELINE	These actions are ongoing.

GOAL 5	Support an efficient and effective transit system
ACTIONS	5-A: Support the development of real-time information and better connectivity for regional transit and local and feeder bus service.
	5-B: Promote coordination of transfer times among Express bus, feeder bus, BART, and park-and-ride lots.
	5-C: Support the expansion of BART service and BART station and parking facilities.
	5-D: Support the construction and maintenance of accessible bus stops, park-and-ride lots, and transit hubs.
	5-E: Support improvements that increase the efficiency of local transit on Regional Routes.
	5-F: Support increased access to BART stations for buses and other alternative modes.
	5-G: Support innovative approaches to improve the efficiency and effectiveness of transit services for seniors and disabled persons through the allocation of Central County's Measure J \$10 million for Additional Transportation for Seniors and People with Disabilities. These funds are in addition to Measure J Other Countywide Programs and total \$35 million in Central County.
	5-H: Support expansion and use of park-and-ride facilities using Express and local buses.
RESPONSIBLE AGENCIES	TRANSPAC and its jurisdictions
TIMELINE	These actions are ongoing.

GOAL 6	Increase participation in the 511 Contra Costa Program to improve multi-modal mobility and decrease single-occupant vehicle use in Central County
ACTIONS	6-A: Support the 511 Contra Costa Program to educate and encourage Contra Costa residents, students and commuters to use multi-modal alternatives by promoting transit, shuttles, carpooling, vanpooling, walking, bicycling, alternative work schedules, and telecommuting.
	6-B: Develop TDM programs at K-12 schools and colleges to encourage carpooling, transit ridership, walking, and bicycling.
	6-C: Promote alternative work opportunities including employer pre-tax benefit programs, compressed work-week schedules, flex schedules, and telework.
	6-D: Encourage commuters to make local trips or trips linked to transit by walking, bicycling, or carpooling instead of driving alone.
	6-E: Promote park-and-ride lot use to potential carpoolers, vanpoolers, and transit riders, including shuttle services, where applicable.
	6-F: In cooperation with Central County jurisdictions, develop TDM plans and provide consultations to improve mobility and decrease parking demand for new development and redevelopment.
	6-G: Explore innovative new technologies to improve mobility and reduce SOV trips.
	6-H: Seek funding to provide bicycle parking infrastructure at employment sites and activity centers throughout Central County.
	6-I: Encourage "green" commuting, including ZEV and NEV vehicles, clean fuel infrastructure, and car sharing.
RESPONSIBLE AGENCIES	511 Contra Costa, TRANSPAC, and TRANSPAC jurisdictions
TIMELINE	These actions are ongoing.



5

TRANSPORTATION AND CIRCULATION

5.1 TRANSPORTATION AND CIRCULATION

The Transportation and Circulation Element is intended to provide guidance and specific actions to ensure the continued safe and efficient operation of the City of Concord’s circulation system. The Element is based on a fundamental philosophy that traffic conditions in the City can be managed through a comprehensive program of transportation planning, land use planning, and growth management strategies. This Element includes provisions for vehicular, transit, aviation, maritime, pedestrian, and bicycle transportation modes.

The Transportation and Circulation Element responds directly to the Concord Municipal Code, which states, “The Transportation Element of the General Plan and studies in conjunction with future development potential in the City have identified improvements that are necessary to the City’s transportation system. As development occurs, improvements to the transportation system must be assured through a planned program of roadway improvements and funding mechanisms.”

State law recognizes that circulation and land use are closely related and requires that policies in this Element and the Land Use Element be linked. Careful integration of the City’s traffic and circulation policies with its land use policies will ensure that there is sufficient roadway capacity to accommodate traffic generated by planned future development. The City is committed to designing a system of regional routes, local roads, public transit, and bicycle and pedestrian routes that will enhance the community and protect the environment.

The Transportation and Circulation Element includes policies related to the physical framework for development that the circulation system is designed to serve, and includes policies for the airport and port environs.

Caltrans is responsible for planning, design, construction, and maintenance of all State highways. Three State highways pass through Concord: I-680, SR 4 and SR 242. Caltrans' jurisdictional interest extends to improvements to these roadways at the interchange ramps serving area freeways. Any Federally funded transportation improvements are subject to review by Caltrans staff and the California Transportation Commission.

MTC is the regional organization responsible for prioritizing transportation projects in a Regional Transportation Improvement Program (RTIP) for federal and state funding. The process is based on evaluating each project for need, feasibility, and adherence to TEA-21 policies and the local Congestion Management Program (CMP). The CMP requires each jurisdiction to identify existing and future transportation facilities that would operate below an acceptable service level and provide mitigation where future growth would degrade that service level.

5.3 GOALS, PRINCIPLES, AND POLICIES

GOAL T-1: A SAFE AND EFFICIENT MULTI-MODAL TRANSPORTATION SYSTEM

Vehicular Circulation

Principle T-1.1: Provide an Easily Accessible, Functional, and Attractive Circulation Network.

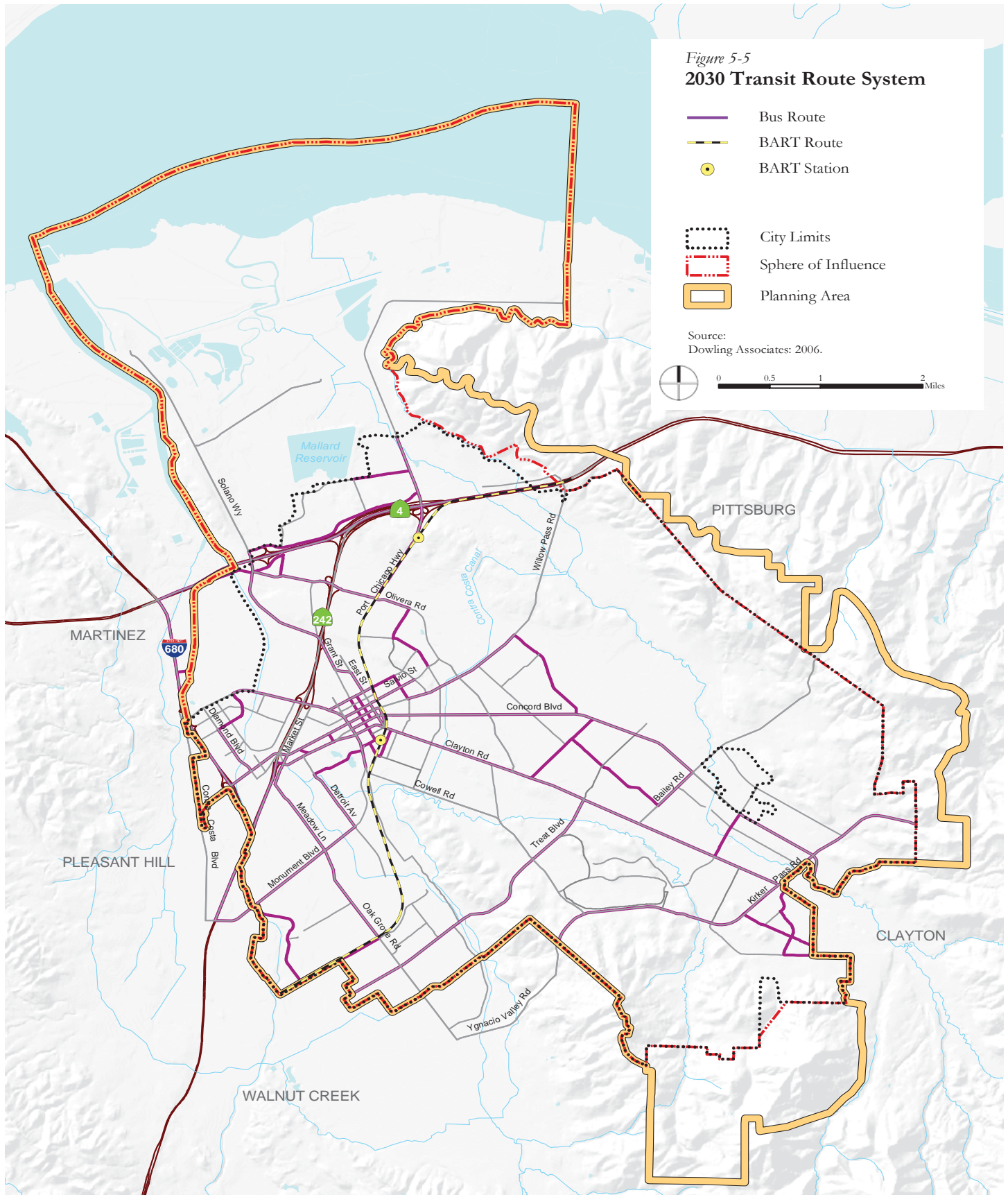
Policy T-1.1.1: Maintain streets at optimal levels to provide safe and efficient travel.

Policy T-1.1.2: Continue to promote a wide variety of transportation alternatives and modes to serve all residents and businesses to enhance the quality of life.

Policy T-1.1.3: Maintain and upgrade transportation systems to provide smooth flow of traffic, minimize vehicle emissions, and save energy.

Policy T-1.1.4: Provide that the level of service at intersections may be exceeded for new development within one-half mile of a BART station, or within one-quarter mile of a transit corridor, where appropriate.

This policy implements the State law that allows for creation of Infill Opportunity Zones where development would be exempt from level of





Policy T-1.3.4: Coordinate with Caltrans and transit providers to identify and implement Park and Ride sites.

Public Transit

Principle T-1.4: Promote a Well-Integrated and Coordinated Transit Network.

Policy T-1.4.1: Coordinate with public transportation agencies to facilitate safe, efficient, and convenient access to transit.

The City’s “Redevelopment Strategy and Implementation Action Plan” adopted in December 2000, identifies shuttle services to enable easy, convenient access to regional shopping areas, such as The Willows and Sunvalley Mall. The City can work with local transit providers to encourage “small-scale” transportation alternatives, such as a jitney, that can provide connections between BART stations, bus stops, parking structures, and nodes of commerce throughout Concord, including the North Concord business area.



Policy T-1.4.2: Work with public transportation agencies to ensure adequate transit service.

The City works with CCTA and BART to ensure equitable transit service is provided to residents and businesses.

Pedestrian Circulation

Principle T-1.5: Provide Safe and Convenient Pedestrian Circulation.

Policy T-1.5.1: Plan linkages to minimize walking distance and enhance the pedestrian circulation.

Foster opportunities for pedestrian circulation throughout the City.

Policy T-1.5.2: Use innovative and effective walkway features to enhance the pedestrian environment.

Examples include wide sidewalks, upgraded hardscapes, decorative crossways, signalized crossings, bulb-outs, and street lighting.



Policy T-1.5.3: Facilitate pedestrian circulation near high activity centers.

Policy T-1.5.4: Encourage new development to provide pedestrian connections to adjacent open spaces, and trails.

Policy T-1.5.5: Identify critical deficiencies in the City's pedestrian circulation system and implement strategies, actions, and funding programs to address them.



This will be done in concert with the Safe Routes to Schools program, the Capital Improvement Program and the Transportation Improvement Program, with priority given to pedestrian circulation improvements that will enhance pedestrian safety and promote walkability.

Bicycle Network



Principle T-1.6: Provide a Safe and Comprehensive Bicycle Network.

Policy T-1.6.1: Implement strategies and actions for enhanced bicycle circulation throughout the City.

The City's Trails Master Plan establishes a specific program for the City to follow.

Policy T-1.6.2: Require provision of bicycle facilities in new developments, where appropriate.

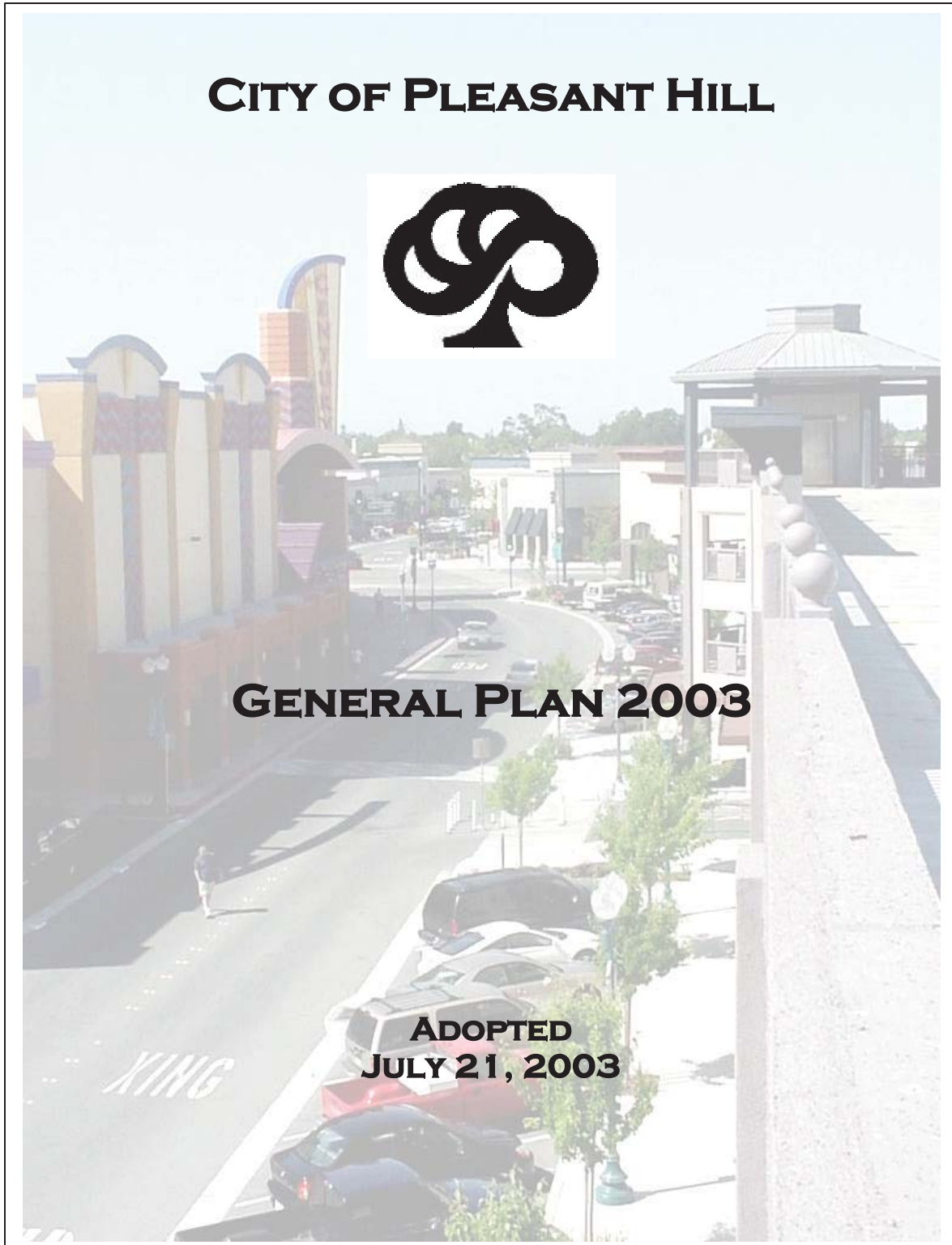
Examples include weather protected bicycle parking and direct and safe access for pedestrians and bicyclists to adjacent routes.

Policy T-1.6.3: Encourage transit operators to provide adequate bicycle accommodations.

Policy T-1.6.4: Encourage new development to provide bicycle access to parks, schools, and transit stops in the design of new residential neighborhoods.

Aviation System

Principle T-1.7: Support the Preservation and Expansion of Aviation Facilities in the Region-Serving Transportation System.



include:

- ✍ widening and realigning Buskirk Avenue and adding traffic signals at its intersections with Hookston Road and/or Mayhew Way to accommodate redevelopment of the Contra Costa Shopping Center (which could increase traffic by 10-15 percent along Buskirk Avenue south of Monument Boulevard);
- ✍ widening Contra Costa Boulevard at Gregory Garden Shopping Center; and
- ✍ adding a traffic signal on Pleasant Hill Road in conjunction with additional development of the Mangini/Delu property.

Due to high volumes, complex configurations and freeway access, the intersections in the city with the highest accident frequency are:

- ✍ Contra Costa Boulevard/Chilpancingo Parkway/Concord Avenue;
- ✍ Monument Boulevard/Buskirk Avenue/Ramona Drive/Lisa Lane;
- ✍ Contra Costa Boulevard/Gregory Lane; and
- ✍ Contra Costa Boulevard/Monument Boulevard.

A February 2001 study of peak hour (4-6 p.m.) traffic on Contra Costa Boulevard indicates:

- ✍ It takes about 12 minutes to drive north from Oak Park Boulevard to Chilpancingo Parkway (about half spent at signals) and 10 minutes to drive south from Chilpancingo Parkway to Oak Park Boulevard (about one-third spent at signals).
- ✍ Average free-flow speed is about 31 mph northbound and about 28 mph southbound, while average overall speed (including stops) is about 16 mph northbound and 19 mph southbound.

Viking Drive and Boyd Road experience substantial congestion associated with morning school-related traffic. Viking Drive provides access to Valley View Middle School, College Park High School and Diablo Valley College, while Boyd Road provides access to Sequoia Elementary and Middle Schools.

Alternate Transportation

Bike routes are designated along most major and local streets in the city, as well as on trails along the Contra Costa Canal, the East Bay Municipal Utility District right-of-way and the railroad tracks ("Iron Horse" trail; see the Circulation System map). The routes link destinations in the city to each other and to the County bikeway system to accommodate both work and recreation-related trips. Pedestrian access also exists on bicycle off road routes.

County Connection provides bus service in the city and to adjacent areas (see the Circulation System map). Public bus routes cover most major streets and serve the Martinez AMTRAK station and the Pleasant Hill BART station for regional commuting.

BART provides a significant alternative to the automobile for work-related trips from Pleasant Hill to the East Bay and San Francisco. BART is expanding its service to South San Francisco and the San Francisco International Airport. The Pleasant Hill Station has about 3,000 parking spaces.

Circulation Goal 1. Establish and maintain a safe and efficient circulation system that emphasizes the use of existing arterial and collector roadways, paths, and bike lanes.

Circulation Policy 1A. Maintain rights-of-way at current widths, except as necessary to relieve specific areas of congestion.

Circulation Program 1.1. Identify specific roadway segments where right-of-way widening, narrowing, or extension may be appropriate or will likely be needed to improve safety.

Circulation Program 1.2. Continue to provide a forum such as the Traffic Safety Committee for citizen input on traffic-related issues.

Circulation Program 1.3. Evaluate intersections with the highest accident rates.

Circulation Program 1.4. Provide roadway improvements necessary to meet the LOS standards established for Basic Routes in the Growth Management Element.

Circulation Program 1.5. Require developers to establish comprehensive construction traffic plans, for approval by City staff, which denote haul routes, detours, and other factors that may impact public safety.

Circulation Goal 2. Decrease traffic delays associated with specific streets and uses.

Circulation Policy 2A. Consider right-of-way widening, signalization, turn and/or parking restrictions, additional turning lanes, and other mitigation measures near schools and other uses with congested conditions.

Circulation Program 2.1. Sponsor forums to obtain citizen input regarding the appropriateness of roadway improvements aimed at reducing local traffic congestion.

Circulation Program 2.2. Evaluate the level of service at intersections that are congested during the peak hour, and develop mitigation measures to alleviate that congestion.

Circulation Goal 3. Reduce speeding, especially in neighborhoods.

Circulation Policy 3A. Focus traffic control efforts in residential areas that experience excessive traffic or speeding.

Circulation Program 3.1. Continue to implement adopted criteria/policies regarding the installation of traffic-calming measures.



Chaucer Drive

Circulation Program 3.2. Undertake traffic-calming measures in identified locations, including around schools as needed.

Circulation Program 3.3. Sponsor forums to obtain citizen input regarding the appropriateness of road improvements intended to reduce speeding.

Circulation Goal 4. Reduce congestion and vehicle trips through non-automobile transportation.

Circulation Policy 4A. Maintain and upgrade the City's bikeway and pedestrian system.

Circulation Policy 4B. Encourage use of bus and rail service for local and regional travel.

Circulation Program 4.1. Identify areas where bikeway connections can be added and/or made safer.

Circulation Program 4.2. Install additional bike lanes, routes, trails and connections where feasible.

Circulation Program 4.3. Work with County Connection to ensure that local bus and shuttle service meets community needs.

Circulation Program 4.4. Explore incentives for public employees to not commute by automobile.

Circulation Program 4.5. Expand use of transit for seniors, students, and persons with disabilities.

Circulation Program 4.6. Work with employers, schools, and developers to encourage ridesharing and transit use.

Circulation Program 4.7. Work with employers, schools, and developers to encourage innovative transportation measures.

Circulation Goal 5. Ensure that streets are safe and pedestrian-friendly.

Circulation Policy 5A. Install or upgrade sidewalks, warning devices, crosswalks, and other pedestrian aids where appropriate.

Circulation Program 5.1. Identify areas where sidewalks, curb cuts, ramps, and other pedestrian amenities should be installed or upgraded.

Circulation Program 5.2. Identify ways that education and police enforcement can improve pedestrian safety.

Circulation Goal 6. Prioritize access and mobility for persons with disabilities.

Circulation Policy 6A. Improve sidewalks to facilitate access by persons with disabilities.

Circulation Program 6.1. Identify specific locations where access for persons with disabilities needs to be improved.

Circulation Program 6.2. Identify grants that may be used to assist in the funding of projects that will improve access for persons with disabilities.



GENERAL
PLAN
2025
CITY OF WALNUT CREEK



April 4, 2006

CHAPTER 5

Transportation

Traffic congestion in Walnut Creek has been a major concern since the end of World War II.

The City has worked to address traffic concerns since the 1950s through new roadways and major road improvements and, in subsequent years, through growth management measures and a transportation demand management (TDM) program. Despite these efforts, traffic congestion continues to be a major concern, particularly downtown and along Ygnacio Valley Road. Downtown attracts people from across the region for business, shopping, dining, and the arts – and most come by car. Ygnacio Valley Road carries Walnut Creek traffic as well as regional traffic to and from Concord, Clayton, and East Contra Costa.



Main Street, view to Mt. Diablo Boulevard, 1951

Through the 1980s, the City's transportation efforts were focused on building new and improving existing roadways. With the adoption of the 1989 General Plan, the City shifted its focus from roadway improvements to better use of the existing transportation infrastructure and programs that would decrease de-

pendency on single-occupant vehicles. Operational improvements were made to the transportation infrastructure, TDM programs were implemented, and transit use was promoted.

Regional planning efforts, such as *Shaping Our Future* in 2002, have stressed managing the transportation system as essential to improving quality of life. *General Plan 2025* promotes the coordination of transportation and land use and the consideration of all travel modes -- transit, bicycling, and walking – in future planning efforts.

This chapter has five sections: Regional Networks, Local Networks, Transit, Transportation Demand Management (TDM), and Downtown Mobility.

REGIONAL NETWORKS

The regional transportation networks comprise both a system of highways and major roadways (routes of regional significance) and a system of regional trails.

Walnut Creek is located near the center of Contra Costa County at the interchange of State Route 24 and Interstate 680. I-680 links central Contra Costa County from north to south and beyond; SR 24 links central county and Walnut Creek with the Lamorinda communities, Oakland, and San Francisco to the west.

In addition to the freeways, Ygnacio Valley Road, Treat Boulevard, Geary Road, North Main Street (from I-680 to the north city limits), and Pleasant Hill Road are identified as

- Policy 6.4. Facilitate use of public sidewalks and walkways throughout the city.** (See Chapter 2, Policy 8.1.)
- Action 6.4.1. Review all projects, programs, and services for accessibility by pedestrians of all abilities.
- Action 6.4.2. Ensure that new parking lots in commercial and multifamily housing developments provide safe and direct paths to building entrances from sidewalks and on-site parking areas.
- Action 6.4.3. Confirm that developments are providing access in accordance with Title 24, California Administrative Code.
- Action 6.4.4. Update the City's ADA Transition Plan, in accordance with the latest requirements of Title 24, California Administrative Code, and the Americans with Disabilities Act.

TRANSIT

Transit services in Walnut Creek include Bay Area Rapid Transit (BART) trains and County Connection buses, including a downtown shuttle bus system and paratransit services for the elderly and those with disabilities. Figure 5 shows the BART and County Connection facilities as of 2005.

BART provides train service to the central San Francisco Bay Area (and direct service to San Francisco) from the Walnut Creek and Pleasant Hill BART stations. Located at California Boulevard and Ygnacio Valley Road, the Walnut Creek BART station is closest to downtown. As of 2005, the station had 5,646 daily entries and the second highest morning entries within the BART system. The majority of rid-

ers are from Alamo, Danville, San Ramon, and Walnut Creek.

Pleasant Hill BART, located at Treat Boulevard and Oak Road, serves Walnut Creek's northern neighborhoods and is easily reached via Geary Road and Treat Boulevard. In 2005, this station had an average 7,206 daily entries and the highest number of morning entries in the system. Riders using this station come from Benicia, Clayton, Concord, Martinez, Pleasant Hill, and Walnut Creek.

Together, the two stations have 5,907 parking spaces. County Connection provides bus feeder services to both stations. Each station also provides lockers and storage for bicycles, wheelchairs, and mopeds.



Walnut Creek BART station

COUNTY CONNECTION

Since 1980, the Central Contra Costa Transit Authority (CCCTA) has operated County Connection buses serving Walnut Creek and other neighboring communities. In 2005, Walnut Creek had 15 local and express County Connection routes, as shown in Figure 5. The routes primarily cover major roads linking the residential neighborhoods of Walnut Creek and adjacent communities with downtown Walnut Creek and the Walnut Creek and Pleasant Hill BART stations.

DOWNTOWN FREE SHUTTLE SERVICE

The downtown free shuttle (County Connection Route 104) provides service between the Walnut Creek BART station and downtown. (See Figure 5, page 5-18.) The shuttle primarily travels north on North Main Street and south on California Boulevard and Locust Street and makes 23 stops. The City reimburses County Connection for fares and enhanced service levels. Weekday ridership averages 780 passengers. Weekend ridership averages 500 on Saturdays and 250 on Sundays.



Downtown free shuttle bus

PARATRANSIT

CCCTA operates the LINK paratransit service in Central County. Most LINK trips are medical or work related, although the service also supports school children and includes early morning, late evening, Saturday, and Sunday service, added in 2000 to comply with requirements of the Americans with Disabilities Act.

From 2000 to 2005, LINK ridership increased by almost 25 percent, from 112,963 trips to 135,213 per year. The largest portion of this increase was attributed to school trips.

CCCTA has concluded that the increase in urban sprawl countywide will limit the overall service ability of LINK in the future.

GOAL 7

Increase transit ridership and service to employment, schools, shopping, and recreation.

- Policy 7.1** Encourage coordination among transit agencies in facilitating connections and transfers while minimizing delay and inconvenience.
- Policy 7.2.** Encourage improvements to transit systems that connect Walnut Creek residents to regional locations.
- Policy 7.3.** Link high-density residential developments, schools, employment centers, and shopping areas via transit.
 - Action 7.3.1.** Work with the Central Contra Costa Transit Authority (CCCTA) to ensure frequent, peak-hour transit services, including express bus, to Walnut Creek schools, employment and activity centers, and park-and-ride lots.
 - Action 7.3.2.** With CCCTA, develop transit incentives and partnerships with the schools and major employment and activity centers.
 - Action 7.3.3.** Continue to financially support the CCCTA with State transportation development act (TDA) and other funds.
 - Action 7.3.4.** Work with CCCTA to provide service to the transit-dependent.

- Action 7.3.5. Working with CCCTA, seek outside funding for improving all forms of bus service in the city.
- Action 7.3.6. Work with CCCTA and the Golden Rain Foundation to provide transit services that connect Rossmoor with the rest of Walnut Creek.
- Action 7.3.7. Arrange for easy availability and prominent display of Walnut Creek bus routes, schedules, and maps, particularly in downtown.
- Policy 7.4. Offer support and funding for effective transit alternatives such as trolleys and improved shuttle services.**
- Action 7.4.1. Consider using a portion of parking revenues (beyond costs) to underwrite local transit improvements.
- Action 7.4.2. Research alternative transit options for the downtown, such as smaller more flexible buses or pedicabs.
- Policy 7.5. Develop a comprehensive plan with CCCTA to install public transit amenities such as benches, passenger shelters, and walkways.**
- Action 7.5.1. Regularly review the locations of bus stops and encourage removal of those no longer in use by CCCTA.
- Action 7.5.2. Work with CCCTA to ensure that bus-stop locations meet both the convenience, comfort, and safety needs of riders and roadway and traffic-flow considerations.
- Action 7.5.3. Promote transit ridership by providing funding assistance for bus turnouts, bus shelters, benches, and sidewalks.
- Action 7.5.4. Require, where appropriate, that new developments provide transit amenities as a condition of project approval.
- Action 7.5.5. Consider options for future transit use when designing new or modifying existing roadways.
- Action 7.5.6. Working with CCCTA, use technology to improve customer service (e.g., real-time arrival signage at bus stops).
- Policy 7.6. Encourage provision of a variety of transportation services for seniors and members of the public unable to use conventional transit.**
- Action 7.6.1. Working with CCCTA, support paratransit services such as the LINK van service.
- Action 7.6.2. Support the continued use of discount transit fares for those in need.
- Action 7.6.3. Continue the senior center volunteer minivan program.



Application Form

Measure J Transportation for Livable Communities (CC-TLC) Program Funds

1. PROJECT INFORMATION

Project Name..... Central Contra Costa Trail Crossing Safety Improvement Study

Project Location Within TRANSPAC subregion of Contra Costa County

Sponsor..... Contra Costa County / East Bay Regional Park District

Implementing agency Same

Partner agencies Contra Costa County and East Bay Regional Park District

Contact for project..... Jim Townsend, Trails Development Manager, EBRPD

jtownsend@ebparks.org / 510-544-2602

2. FUNDING

In thousands of dollars, please provide the estimated cost of the proposed project, the amount of CC-TLC funds requested, and any other funds that have been committed to the project.

<i>Source</i>	<i>Amount</i>
Total project cost †	\$50,000
Requested CC-TLC funds	\$50,000
Other committed funding 1 ‡	\$000
Other committed funding 2 ‡	\$000
Unfunded balance	\$100,000

† Minimum request is \$50,000

‡ Add other committed funding sources as needed

3. POTENTIAL PHASING

As this project is a Safety Improvement Study, it will not be phased. Implementation of recommended crossing improvements could be considered a future phase of the project.

4. PROJECT DESCRIPTION

The Central Contra Costa County Trail Crossing Safety Improvement Study will examine strategies to improve safety at mid-block regional trail crossings in Central Contra Costa County.

The study will examine existing conditions and use patterns at mid-block trail crossings to determine if additional safety measures are warranted at up to ten crossings. The study will include a field review of crossing geometry and speed limits, review collision and accident data. Intercept surveys of pedestrians and cyclists at the crossings will be conducted during both AM and PM periods and on weekends. The data will be reviewed to identify crossing safety issues and prepare concept plans for potential solutions. Information on existing crossing treatments, including passive solutions such as striping and signage, and active treatments such as user-activated and loop detector signals and other types of lighting will be collected, and the cost effectiveness of various treatments will be examined. The study will develop an implementation strategy and cost estimates for achieving the recommended crossing safety improvements.

A critical component of this effort will be the user surveys. Anecdotal information indicates that collision data and police reports do not tell the whole story when it comes to user's perceptions of crossing safety. The *perception* of an unsafe condition can be enough impact use of the trail network for transportation purposes.

5. PROPOSED SCHEDULE

In the following table, enter the expected beginning and ending *month and year* for each of the phases listed. If the phase has been finished, enter "Completed" in the "To" column.

<i>Phase</i>	<i>From</i>	<i>To</i>
Crossing Safety Study	April, 2012	October, 2012
Design	N/A	N/A
Environmental Clearance	N/A	N/A
Right-of-Way	N/A	N/A
Construction	N/A	N/A

6. ABILITY TO MEET CRITERIA

Describe how the proposed project would meet the following four criteria:

<p>Achievement of CC-TLC Goals: Identify which of the six goals of the CC-TLC program the proposed project would help achieve — a project need not meet all of them — and <i>briefly</i> describe how it would do so (see Exhibit One for these six goals)</p>	<p>By developing strategies to improve the safety of pedestrians and cyclists at mid-block trail crossings, the project will:</p> <ul style="list-style-type: none"> • Help create walkable, pedestrian-friendly neighborhoods and business districts • Provide for a variety of transportation choices to enhance the community's mobility
<p>Feasibility: Describe why the project is feasible and any outstanding issues in the project development process — design, environmental clearance, right-of-way purchase, and PS&E — or funding of the project</p>	<p>This project is a study, providing cost estimates and strategies to improve trail crossing safety. Implementation of the strategies will require additional funding.</p>
<p>Local and policy support: Identify local policies that support the project and the integration of the project with other local efforts, as well as other support from the general public, the RTPCs and other relevant agencies</p>	<p>East Bay Regional Park District has received requests from elected officials and the public to examine ways to improve trail crossing safety. This proposal is the result of those requests.</p>
<p>Matching funds: Identify any funds from other sources that are or would be committed to the project</p>	<p>There are no matching funds allocated to this study.</p>

EXHIBIT ONE

Goals of the Measure J CC-TLC Program

Measure J establishes six goals for the Measure J Transportation for Livable Communities program.

- Help create walkable, pedestrian-friendly neighborhoods and business districts
- Promote innovative solutions, including compact building design and context-sensitive site planning that is integrated with the transportation system
- Help create walkable, pedestrian-friendly access linking housing and job centers to transit
- Help create affordable housing
- Encourage a mixture of land uses and support a community's development or redevelopment activities
- Provide for a variety of transportation choices to enhance a community's mobility, identity, and quality of life



Application Form

Measure J Transportation for Livable Communities (CC-TLC) Program Funds

1. PROJECT INFORMATION

Project Name CENTRAL CONCORD PEDESTRIAN IMPROVEMENTS & STREETScape PROJECT

Project Location VARIOUS STREETS IN CENTRAL CONCORD INCLUDING, CLAYTON ROAD, WILLOW PASS ROAD BETWEEN MARKET STREET AND GALINDO STREET AND THE DOWNTOWN AREA INCLUDING GRANT STREET AND SALVIO STREET

Sponsor CITY OF CONCORD

Partner agencies CCTA

Contact for project... DANEa GEMMELL
 CITY ENGINEER
 1950 PARKSIDE DR, M/S 40
 CONCORD, CA 94519
danea.gemmell@ci.concord.ca.us
 (925) 671-3470

2. FUNDING

In thousands of dollars, please provide the estimated cost of the proposed project, the amount of CC-TLC funds requested, and any other funds that have been committed to the project.

<i>Sthece</i>	<i>Amount</i>
Total project cost †	\$3,535,000
Requested CC-TLC funds	\$3,000,000
Other committed funding 1 ‡	\$535,000
Other committed funding 2 ‡	\$0
Unfunded balance	\$0

† Minimum request is \$50,000

‡ Add other committed funding stheces as needed

3. POTENTIAL PHASING

The proposed project has been packaged with several Phase Components that can be evaluated by the TRANSPAC Grant Evaluation Committee as an A la Carte Menu. Concord requests that the full project scope be considered as the Phase Components are designed to work together for an integrated Transportation for Livable Community project. The Phase Components are detailed in the Project Budget (Exhibit 3 – Phased Summary Budget, and Exhibit 4 – Detailed Budget) and include: (1) Clayton Road Pedestrian Safety Upgrades; (2) Willow Pass Road Pedestrian and Roadway Improvements; and (3) Downtown Streetscape. Additionally, individual features are clearly identified within each component and can be removed if funding is not available.

4. PROJECT DESCRIPTION

The **Central Concord Pedestrian Improvements and Streetscape Project** will enhance pedestrian and bicyclist safety in the immediate areas surrounding the vital Downtown mixed use area and extend the sense of community to BART riders and along Willow Pass corridor, which serves as a gateway into Concord. This project proposes to provide a traffic signal and secure crossings at Clayton Road and Sutter Street that will connect the Monument Corridor community to the bustling business districts along Willow Pass Road. It also provides wider sidewalks, seat walls to accommodate pedestrian respites and colored concrete intersection improvements to provide visibility for pedestrians. Finally the project proposes streetscape enhancements in the Downtown area including bicycle signage and storage facilities on Grant Street near BART, cost-effective crosswalk upgrades and repairs, wayfinder kiosks, additional trash and recycling receptacles and safety and atmosphere lighting. The three separate phases are interrelated and will provide pedestrian-friendly transportation options throughout the City that will enhance the quality of life of Concord residents.

Project Purpose

Concord is the largest city within Contra Costa County and is home to almost 125,000 residents. This major metropolis is the largest business center in the County with more than four million square feet of class A office space, more than five million square feet of industrial space, and nearly 700,000 square feet of Research and Development space. The City is committed to providing a variety of transportation choices to enhance the community's mobility, which includes over 17 miles of Regional Trails, an airport, two BART stations, many local mass transit options, and is served by Interstate 680, and State highways 4 and 242.

The focal point of Downtown Concord is Todos Santos Plaza, which encompasses an entire city block and is host to more than 200,000 attendees annually at the Farmer's Market (which has an average attendance of 5,000 in a single night), free summer

concerts, special events, and many surrounding restaurants. Much of the area immediately around Downtown has recently been redeveloped with new high-density apartment and condominium projects, which take advantage of the close proximity to public transportation and the vibrant area surrounding the park.

Through the Redevelopment Agency, Concord implemented a Street Beautification Master Plan for the Central Concord area in 1994, which includes Willow Pass Road and Bonifacio Street on the north-end, Clayton Road and Park Street on the south-end, Market Street on the west-end, and East Street on the east-end. This streetscape beautification plan was developed to enhance and express the interaction between the “circulation” (traffic flow, transit connections, etc) and the “land use character” (landscape development, safety features, etc) in the Central Concord area. Streetscape projects were implemented to revitalize the Downtown area in 1995. However, since that time, the City has encouraged local businesses to assist in the implementation of further enhancements such as façade improvements, installation of wayfinder signage, and the promotion of special event programs to draw visitors to the area.

The nearby business district is home to more than 4,000 employees in the Bank of America Technology Center and One Concord Center buildings which are occupied by several national corporations, as well as numerous educational opportunities, such as John F. Kennedy University. Many neighboring retail and shopping centers – including fine dining and kid-friendly restaurants – allow locals to walk to their favorite after-work destinations. The proposed Central Concord Pedestrian Improvements and Streetscape Project continues the strategies implemented in the Master Plan by building on that very same sense of community, while also providing infrastructure improvements to enhance the existing architecture and create a walkable, pedestrian-friendly business district that is linked to multiple transportation options and high density housing in this mixed-use area.

The City of Concord is a firm believer in the health benefits derived from walking and bicycling and many of the project elements are designed to support this fact. The first priority is to provide a safe, walkable environment for the residents to enjoy, specifically those in the Todos Santos Town Center and Retail/Entertainment Overlay Districts. These communities include many high density, affordable housing, residential areas and several major shopping destinations, such as, Sunvalley Mall, the Willows Shopping Center, Park & Shop Center, and the Downtown’s specialty retail and dining establishments. The second priority is to expand the streetscape image and identity of Downtown along Willow Pass Road and Clayton Road which serves as gateways into the City. Many of the businesses and retail centers are showing signs of age and need to be revitalized as they serve as destinations for nearby mixed-use buildings and neighborhoods. The proposed **Central Concord Pedestrian Improvements and Streetscape Project** will address both priorities by enhancing the facilities in the Downtown area and expanding them into the older Gateway corridors with a number of features (Exhibit 1).

PROJECT FEATURES

1. Clayton Road Pedestrian Safety Upgrades
 - a. Install a traffic signal at the Clayton Rd/Sutter St intersection.
 - b. Install crosswalks at the Mira Vista Terrace/Concord Blvd/Clayton Rd intersection (including an island pedestrian pathway and associated signage).
2. Willow Pass Road Pedestrian Improvements (Market Street to Galindo Street)
 - a. Replace north-side sidewalk between Sutter St and Gateway Blvd (including grade separation and installation of pedestrian seatwalls).
 - b. Replace crosswalks at five intersections (including installation of new ADA-compliant, decorative crosswalks).
 - c. Replace the remaining roadway on Willow Pass between intersections to enhance traffic flow and keep integrity of pavement.
3. Downtown Streetscape Improvements
 - a. Install additional 2 Wayfinder Kiosks along Grant St from the Concord BART station into the Downtown area.
 - b. Rehabilitate crosswalks at 27 intersections (including ADA-compliant, decorative concrete staining upgrades).
 - c. Provide bicycle facilities (including installation of a Class 3 bike route signage along Grant Street and Salvio Street with installation of 8 new bike lockers near the Concord BART station.)
 - d. Safety/Atmosphere Lighting (including installation of twinkle lights in the pre-wired trees along Grant St. from the Concord BART station into the Downtown area).
 - e. Streetscape Furniture Rehabilitation (including installation of upgraded trash/recycling depositories and refinishing of streetlight poles along Grant St. from the Concord BART station into the Downtown area).

Clayton Road Pedestrian Safety Upgrades

Traffic Signal – Clayton Road is a major thoroughfare in this area which leads traffic east and speeding into town from Hwy 242. Over the last 5 years, more than 600 traffic accidents have occurred resulting in over 200 injuries or fatalities in the 0.5 mile stretch on Clayton Rd from Hwy 242 to Galindo St, of which almost fifty included pedestrians and/or bicyclists (Exhibits 5 & 6). This project includes the installation of a traffic signal, which meets the warrants, to safely guide pedestrians and bicyclists across the widest intersection on Clayton Rd at Sutter St, which crosses 7 lanes of traffic and runs almost 100 feet (Exhibit 2, Photos 1 & 2).

Crosswalks & Pathway – One block to the east at Mira Vista Terrace, the Clayton Road/Concord Blvd loop is created between two large islands that separate almost 10 lanes of traffic flowing east on Clayton Road and west on Concord Blvd. There is no crosswalk present and a decorative gate erected along Concord Blvd creates a severe site-distance impediment for pedestrians and bicyclists who often dart across this dangerous intersection jumping from street to island, and back again (Exhibit 2, Photo 6). The project includes the creation of a pathway along the length of the smaller island and the installation of two crosswalks. We will create an asphalt pathway along the length of the smaller island which will provide a connection to the aforementioned signalized crosswalk at Clayton Rd/Sutter St, to a new crosswalk at the tip of the island, connecting pedestrians to the north-side of Clayton Rd (Exhibit 2, Photos 3-5). The second crosswalk will begin approximately 15 feet east of the Mira Vista Terrace and Concord Blvd intersection, and cross Concord Blvd to the larger island (Exhibit 2, Photo 6). To increase pedestrian and bicyclist safety, we will remove more than 100 feet of the decorative fence along Concord Blvd, to provide improved site-distance to allow vehicles to see all crosswalk users. Additionally, speed reduction and pedestrian crosswalk signs will be installed to apprise drivers of possible pedestrians or bicyclists in the road.

Willow Pass Pedestrian Improvements

Sidewalk Replacement – One block to the north, the Park & Shop Retail Center occupies the length of the north-side of Willow Pass Road from Gateway Blvd on the west end, to Mira Vista Terrace on the east. Anchored by the Fry's Electronics Store, Park & Shop is booming with unique storefront shops, grocery stores, restaurants and services. Park & Shop is a high volume destination for nearby Monument Corridor residents who commonly travel on foot or bicycle with small children. This project will replace the current sidewalk that runs almost the length of Park & Shop (Gateway Blvd to Sutter St) between the parking lot and the public right-of-way, which has steep grading issues, meanders in and out of trees, streetlight poles and previously installed landscaping, and has many safety and/or uplifted concrete trip hazards from tree roots (Exhibit 2, Photos 11 & 12). The new sidewalk will widen the path of travel from 4 feet to at least 8 feet where feasible, and will feature pedestrian seatwalls on the parking lot side of the path to help compensate for any severe grading issues and to provide a rest area for pedestrians and cyclists, especially those with small children.

Decorative Colored Crosswalks – This section of Willow Pass Road is also home to a thriving business community, including T-Mobile, Kyocera, PG&E, Fidelity National Home Warranty, and Harris and Associates in the Gateway Towers buildings, which employs nearly 2,000 citizens. The Park & Shop Retail Center, as well as many nearby Todos Santos/Downtown area shops and restaurants, are the lunchtime and after-work destination for the employees of these businesses and surrounding communities. To continue to provide an increase of pedestrian-friendly options in the area, the project will remove and replace several rundown, faded, pothole-ridden crosswalks with new ADA-compliant, color concrete crosswalks, as a safety feature to promote high visibility at five

main intersections along Willow Pass Rd (Mira Vista Terrace, Sutter St, Fry Way, Gateway Blvd and Market St) (Exhibit 2, Photos 13 & 14).

Roadway Improvements – After the five intersections are improved through construction of the colored crosswalks, the remaining roadway on Willow Pass between Market St and Galindo St, will need to be addressed. In 2009, a pavement condition study with deflection testing was completed which shows that these pavement areas require rehabilitation. With the disruption to businesses and pedestrians caused by construction of the sidewalk and crosswalk improvements, the City recommends minimizing the time for construction and including the remaining roadway improvements in the project. This scope allows for loop detectors to be replaced and signals to be retimed to adjust for pedestrian crossings. Road improvements bring immediate and sometimes dramatic benefits to travelers through improved access to jobs and markets; improved comfort, speed, and safety; and lower vehicle operating costs. Additionally, these improvements will assist in revitalizing the Park & Shop area by investing in and hopefully kick starting, additional façade improvements.

Downtown Streetscape Improvements

Wayfinder Kiosks – The recent Todos Santos/Downtown area renovation created several new pedestrian and visitor friendly elements including wayfinder kiosks. In total, 13 kiosks were placed in central locations around the area to provide maps and directions to various points of interest throughout the Todos Santos/Downtown area. The residents consider the kiosks a tremendous success and the project extends this informative tool into the reaches of Central Concord including the BART station and transit hub. The installation of two additional wayfinder kiosks will continue to further the gateway into the City. The first location is in the public right of way at the Concord BART Station at the intersection of Grant St and Park St, the second location is at Grant St and Clayton Rd (Exhibit 2, Photo 15).

Crosswalk Upgrades – There are many major arterial roadways that intersect the Downtown area and over time, the constant flow of traffic through these intersections has taken its toll on the accompanying concrete crosswalks. Many of the colored concrete crosswalk lines have become worn and faded and are littered with pits and potholes. This provides a safety concern for pedestrians and bicyclists, as well as commuters. The project includes the rehabilitation of approximately 30 intersections throughout the Downtown area, including saw cutting and replacement of all potholes and failed areas. The faded crosswalk lines will be revitalized by staining the concrete to improve safety and visibility and tie into the proposed crosswalks along Willow Pass Road, further expanding the identity of the Downtown area (Exhibit 2, Photos 7 & 8).

Bicycle Facilities – The City of Concord is striving to find new safe and convenient ways to increase the network of pedestrian and bicycle-friendly commuting options to combat the volume of single occupant vehicles on the roadways. Both the 2002 Concord Trails

Master Plan and the more recent Countywide Bike and Pedestrian Plan recommend installation of a Class 3 bike route in the Downtown Area. The route will run along Grant St, leading from the BART station into the Downtown area to Adobe St, and also along Salvio St from East St to Market St. In addition to signage, the project includes 8 bike lockers strategically placed along the route to provide safe storage facilities in the most heavily used areas (Exhibit 2, Photo 16). These routes service not only the business districts, but also several retail centers, schools and government services.

Safety/Atmosphere Lighting – Increasing safety is always a number one concern for the City of Concord. Many residents commute out of the area for work and return home in the often dark, evening hours. Though Grant St currently has pedestrian level lights installed, the large number of mature trees which line the street, block-out much of the light, causing pedestrians to feel uneasy walking in the dark. In an effort to combine a necessary safety feature with the City’s desire to provide a beautiful and uniformed atmosphere for the Downtown area, the project includes the installation of energy efficient twinkle lights in the pre-wired trees that line Grant St, leading from the BART station into the currently illuminated Todos Santos Plaza (Exhibit 2, Photos 9 & 10).

Streetscape Furniture Rehab – Keeping the City clean and free from hazardous trash and litter is a major concern for Concord. Through a partnership with the Concord Disposal Service, all trash containers located throughout the Downtown area are emptied on a weekly basis. Unfortunately, at this time, only the Todos Santos Plaza area provides dual trash and recycling receptacles. This project will replace the limited trash-only containers along Grant St, leading from the BART station into Downtown, with the new dual trash and recycling receptacles to complete the uniformity of the Todos Santos/Downtown area, as well as to provide an important example of re-use throughout the community. Additionally, in the same area, decorative low-level streetlights line the street to provide visibility and safety. Over time, these streetlights have become worn and run-down with chipped and cracking paint. To continue the desire to provide a beautiful streetscape, this project includes the refinishing of these streetlights with a fresh paint job.

Project Operation and Maintenance

The operation and maintenance needs for the majority of the project components will be consolidated into the City of Concord’s regular fiscal year program, which will absorb any additional costs. The lighting elements included in the project (Safety/Atmosphere Lighting) will be included in the City of Concord’s fiscal year Landscape and Lighting District program.

5. PROPOSED SCHEDULE

In the following table, enter the expected beginning and ending *month and year* for each of the phases listed. If the phase has been finished, enter “Completed” in the “To” column.

<i>Phase</i>	<i>From</i>	<i>To</i>
Preliminary Design & Planning	November 2011	June 2012
Design	July 2012	June 2013
Environmental Clearance	January 2013	February 2013
Right-of-Way	January 2013	March 2013
Construction	July 2013	May 2014

The Proposed Project Schedule assumes that the CC-TLC Grant is successful. The 12-month design timeline assumes that the design consultant will be selected through a separate procurement process due to the size of the project. No environmental clearance or right of way issues are anticipated. The construction is scheduled for late 2013 to follow a major sewer line/water line replacement project that is scheduled in the Downtown Area, which is being constructed as a partnership between the City and the Contra Costa Water District.

6. ABILITY TO MEET CRITERIA

Describe how the proposed project would meet the following four criteria:

<p>Achievement of CC-TLC Goals: Identify which of the six goals of the CC-TLC program the proposed project would help achieve — a project need not meet all of them — and <i>briefly</i> describe how it would do so (see Exhibit One for these six goals)</p>	<p>This project achieves five of the six goals of the CC-TLC program, with only creating affordable housing not being addressed. Central Concord is fortunate to be a mixed-use area with transit options that link housing to business and retail centers. The traffic signal on Clayton Rd encourages pedestrians from the Monument Corridor to safely walk to job centers and shopping areas, as many do not own cars. This project builds on the strong identification of Downtown and expands it along Willow Pass to enhance this areas identity and help revitalize it. Using the colored crosswalk theme ties both areas aesthetically and provides safety to pedestrians. Additional features will improve lighting, install bike facilities and erect signage. Singularly, each feature undoubtedly benefits the area, however as a group, the features</p>
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become a comprehensive plan that expands an already strong community identity that reaches out on a regional scale to residents and visitors alike.

Feasibility: Describe why the project is feasible and any outstanding issues in the project development process — design, environmental clearance, right-of-way purchase, and PS&E — or funding of the project

The project will build on previous project designs and is primarily located within the City’s right of way. Therefore, while it is not “shovel ready”, it does have all of the “tools out of the shed” and ready to work: (1) Wayfinder Kiosks have already been designed; (2) Recently constructed Colored Crosswalks on Monument Boulevard provide practical feedback for new design; (3) Deflection testing is completed for roadway improvements; (4) Traffic Signal Warrants Analysis for Clayton Road/Sutter St intersection is complete; (5) Power Conduits are already installed for tree lights in Downtown. The project’s Environmental Clearance is expected to be a Notice of Exemption. There is no right-of-way acquisition required and local match funding is committed. (See discussion on Matching Funds below.)

Local and policy support: Identify local policies that support the project and the integration of the project with other local efforts, as well as other support from the general public, the RTPCs and other relevant agencies

This project incorporates the Street Beautification Master Plan which was adopted in 1994 and the Concord Trails Master Plan of 2003 and Countywide Pedestrian and Bicycle Plan Update of 2009. In 2010, Concord adopted an Economic Vitality Strategy that covers the project area. The project is well supported locally and has been discussed with members of the Todos Santos Business Association, and many local businesses and employers who have provided letters of support (Exhibit 7). “Yelp” reviews for Todos Santos Plaza/Downtown area show how well-used the area is to residents and outside visitors. Many frequent multiple businesses by walking through the community after parking. Additional reviews for Park & Shop are also included and demonstrate the need for improvements in an area that aspires to have the same sense of community (Exhibit 8).

Matching funds: Identify any

The City proposes a 15% local match or \$535,000 in

funds from other sources that are or would be committed to the project

gas tax funds that will be committed to the project. The City Engineer is responsible for prioritizing project areas for City Project No. 1173 (Citywide Sidewalk Improvement Annual Program) and Project No. 2157 (Pavement Management Annual Program). Both projects have ten years of planned funding which is adequate to absorb the \$535,000 local match. As part of the FY 2012-13 CIP budget process, the local match will be moved to the proposed project.

The following photographs and documents have been attached as exhibits:

- Exhibit 1: Project Setting Map
- Exhibit 2: Photograph Exhibits
- Exhibit 3: Phased Summary Budget
- Exhibit 4: Detailed Budget
- Exhibit 5: Warrants Analysis and Pedestrian Movement Data for Traffic Signal at Clayton Road and Sutter Street
- Exhibit 6: Collision Data for 2006-2011
- Exhibit 7: Letters of Support from:
Virginia Thomas, President, Todos Santos Business Assoc.;
Vern Phillips, Senior Vice President, Harris & Associates;
Craig Semmelmeier, Principal, Main Street Property Services, Inc.;
Jeffrey Woods, Property Manager, JCM Partners, LLC
- Exhibit 8: Yelp Reviews of Park and Shop and Todos Santos Plaza/Downtown Area

Legend

-  New Crosswalks
-  Rehabilitated Crosswalks
-  Sidewalk
-  Class III Bike Lane
-  Tree Lights
-  Project Limits
-  New Traffic Signal





1. Intersection of Clayton Road and Sutter Street: Proposed improvements include construction of a traffic signal and controlled crosswalks.



2. Intersection of Clayton Road and Sutter Street: Proposed improvements include construction of a traffic signal and controlled crosswalks.

Central Concord Pedestrian Improvements & Streetscape Project



3. Island Median @ Clayton Rd/Sutter St: Proposed improvements include new Pedestrian Path connection from pedestrian refuge @ traffic signal, to new crosswalk location.



4. Island Median @ Clayton Rd/Sutter St: Proposed improvements include new Pedestrian Path connection from pedestrian refuge @ traffic signal to new crosswalk location.



5. East-bound on Clayton Rd (between both islands): Install new crosswalk for pedestrian access from new median island pathway



6. West-bound on Concord Blvd (near Mira Vista Terrace): Install new crosswalk and remove section of decorative fence

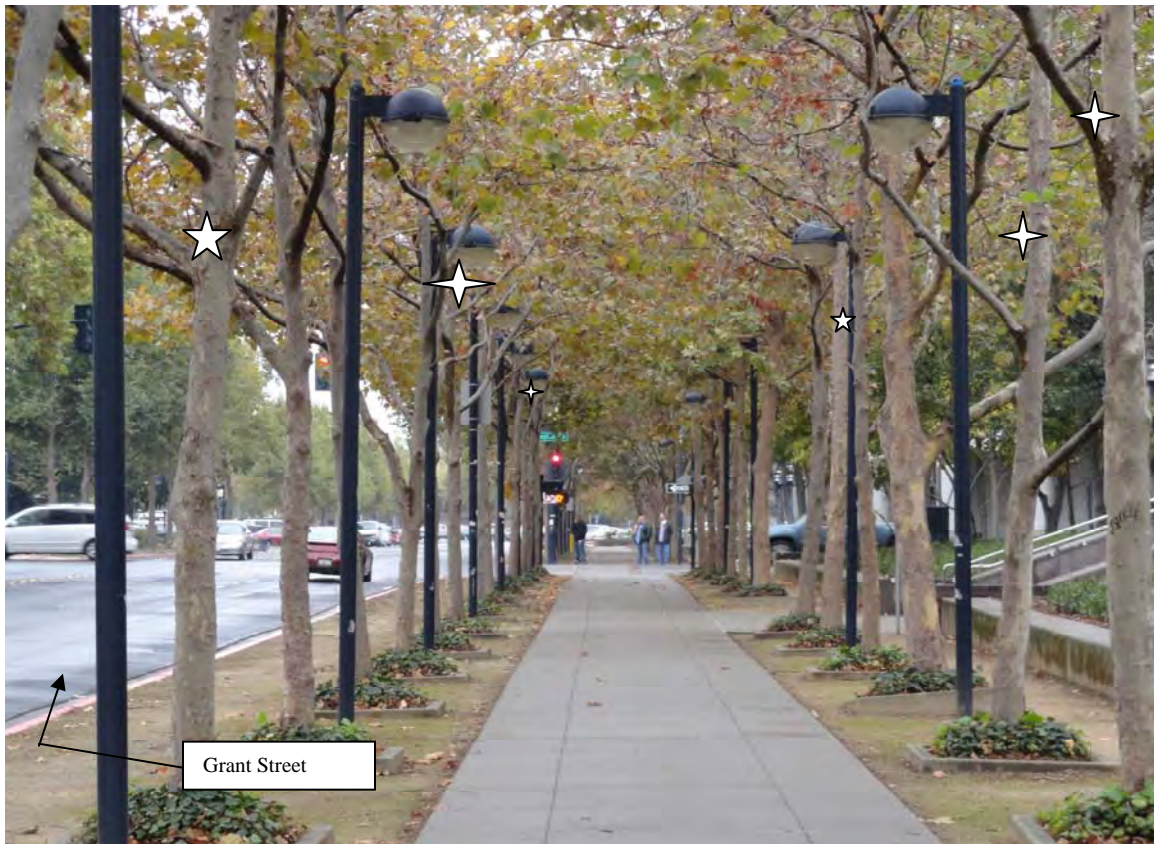
Central Concord Pedestrian Improvements & Streetscape Project



7. Clayton Road @ Grant St: Proposed improvements include replacing existing pothole & crack-ridden crosswalks with ADA-compliant, high-visibility color concrete crosswalks.



8. Concord Blvd @ Grant St: Proposed improvements include replacing existing pothole & crack-ridden crosswalks with ADA-compliant, high-visibility color concrete crosswalks.



9. North-bound on Grant St (from BART station toward Downtown): Proposed improvements include installation of energy-efficient “Twinkle” lights in the pre-wired trees to provide safety & uniformed atmosphere for the Downtown area.



10. South-bound on Grant St (from Downtown toward BART station): Proposed improvements include installation of energy-efficient “Twinkle” lights in the pre-wired trees to provide safety & uniformed atmosphere for the Downtown area.



11. North-side Willow Pass Road: Proposed improvements include widening the sidewalk to 8' where feasible and evening out grade issues, including installation of pedestrian seatwalls where possible.



12. North-side Willow Pass Road: Proposed improvements include widening the existing sidewalk to 8' where feasible, replacing the meandering style through streetlight poles & landscaping and repairing trip hazards due to tree roots.



13. North-side Willow Pass Road @ Fry Way: Proposed improvements include replacing existing crosswalks with ADA-compliant, high-visibility color concrete crosswalks.



14. North-side Willow Pass Road @ Gateway Blvd: Proposed improvements include replacing existing crosswalks with ADA-compliant, high-visibility color concrete crosswalks.

Central Concord Pedestrian Improvements & Streetscape Project



15. Wayfinder Kiosks in Downtown area: Proposed improvements include installation of two additional wayfinder kiosks in the Downtown area to provide maps & directions to various points of interest.



16. Grant St (BART station, through Downtown to Adobe St) and Salvio St (East St to Market St): Proposed improvements include installation of a Class 3 bike route throughout the Downtown area with signage and 8 bike lockers for safe storage.

**Central Concord Pedestrian Improvements and Streetscape
Phased Budget Summary**

	Clayton Road Pedestrian Safety Upgrades	Pedestrian Improvements: Willow Pass (Market Street to Galindo Street)	Roadway Improvements: Willow Pass (Market Street to Galindo Street)	Downtown Streetscape	Total
Engineer's Estimate of Features	\$ 483,440	\$ 736,400	\$ 573,450	\$ 320,920	\$ 2,114,000
Contractor Mobilization	\$ 24,200	\$ 36,800	\$ 28,700	\$ 16,000	\$ 105,700
Traffic Control and Signage/Message Boards	\$ 19,700	\$ 29,500	\$ 22,900	\$ 12,800	\$ 84,900
Water Pollution Control and SWPPP	\$ 4,800	\$ 7,400	\$ 5,700	\$ 3,200	\$ 21,100
Hard Cost Construction Subtotal	\$ 532,140	\$ 810,100	\$ 630,750	\$ 352,920	\$ 2,325,910
10% Contingency	\$ 53,200	\$ 81,000	\$ 63,100	\$ 35,300	\$ 232,600
Construction Inspection (15% of Construction)	\$ 79,800	\$ 121,500	\$ 94,600	\$ 52,900	\$ 348,800
Construction Subtotal	\$ 665,140	\$ 1,012,600	\$ 788,450	\$ 441,120	\$ 2,907,310
Administration: Planning, Coordination, Cost Recovery	\$ 63,900	\$ 97,200	\$ 75,700	\$ 42,400	\$ 279,200
Design including Survey, Plans, Specifications	\$ 79,800	\$ 121,500	\$ 94,600	\$ 52,900	\$ 348,700
Project Budget by Phase	\$ 809,000	\$ 1,231,000	\$ 959,000	\$ 536,000	\$ 3,535,000
Request for Measure J -TLC Grant Funds	\$ 709,000	\$ 1,081,000	\$ 739,000	\$ 471,000	\$ 3,000,000
Local Funds	\$ 100,000	\$ 150,000	\$ 220,000	\$ 65,000	\$ 535,000
Total Funding Allocation Request	\$ 809,000	\$ 1,231,000	\$ 959,000	\$ 536,000	\$ 3,535,000
	12%	12%	23%	12%	15%

**Central Concord Pedestrian Improvements and Streetscape
Detailed Project Budget**

Task Description	Quantity	Unit	Unit Price	Item Total	Total
Clayton Road Pedestrian Safety Upgrades					\$ 483,440
Traffic Signal at Clayton Road and Sutter Street	1	LS	\$ 250,000	\$ 250,000	
Remove and Replace Curb Ramps	4	EA	\$ 3,500	\$ 14,000	
Clearing and Grubbing	1	LS	\$ 8,000	\$ 8,000	
Picket Fence Relocation	1	LS	\$ 2,500	\$ 2,500	
Striping and Pavement Markings	1	LS	\$ 8,000	\$ 8,000	
Utility Adjustment	1	LS	\$ 12,000	\$ 12,000	
Roadway Excavation for PCC Concrete Crosswalk	9120	SF	\$ 2.50	\$ 22,800	
Colored PCC Band & Crosswalk -including AB	3800	SF	\$ 12.00	\$ 45,600	
Asphalt Concrete Paving - 0.5' Plug	5320	SF	\$ 3.50	\$ 18,620	
Stamped Concrete	1500	SF	\$ 14.00	\$ 21,000	
Median Modifications	250	LF	\$ 40.00	\$ 10,000	
Median PCC Trail Improvements	2160	SF	\$ 12.00	\$ 25,920	
Median Landscape Improvements	6000	SF	\$ 7.50	\$ 45,000	
Pedestrian Improvements:					
Willow Pass (Market Street to Galindo Street)					\$ 736,400
Roadway Excavation for PCC Crosswalk & Intersections	46800	SF	\$ 2.50	\$ 117,000	
Colored PCC Band & Crosswalk -including AB	24700	SF	\$ 12.00	\$ 296,400	
Asphalt Concrete Paving - Intersections	1200	Ton	\$ 95.00	\$ 114,000	
Remove Landscape and irrigation	4400	SF	\$ 4.00	\$ 17,600	
Expand Sidewalk	4400	SF	\$ 15.00	\$ 66,000	
Curb and Gutter	160	LF	\$ 40.00	\$ 6,400	
Reconstruct Driveways	5	EA	\$ 10,000	\$ 50,000	
Seat Walls for Grade Separation	300	LF	\$ 50	\$ 15,000	
Curb Ramp	12	EA	\$ 3,500	\$ 42,000	
Red Curb	12000	LF	\$ 1	\$ 12,000	
Roadway Improvements:					
Willow Pass (Market Street to Galindo Street)					\$ 573,450
Remaining Roadway Excavation for Street Integrity	16300	SY	\$ 4.00	\$ 65,200	
Adjust Utilities	26	EA	\$ 500.00	\$ 13,000	
Asphalt Concrete	3750	Ton	\$ 95.00	\$ 356,250	
Loop Detectors and Conduit	114	EA	\$ 1,000	\$ 114,000	
Striping and Pavement Markings	1	LS	\$ 25,000	\$ 25,000	
Downtown Streetscape					\$ 320,920
Crosswalk: Concrete Stain Upgrades	10240	SF	\$ 8.00	\$ 81,920	
Crosswalk: Concrete Patching	2600	SF	\$ 15.00	\$ 39,000	
Recondition Pedestrian Lighting Poles	1	Allow	\$ 30,000	\$ 30,000	
Bike Lockers	8	EA	\$ 3,500	\$ 28,000	
Decorative Trash/Recycling Receptacles	10	EA	\$ 1,000	\$ 10,000	
Grant Street: Tree LED Twinkle Lights	40	EA	\$ 2,500	\$ 100,000	
Grant Street: Wayfinder Kiosks	2	EA	\$ 12,000	\$ 24,000	
Bicycle Route Signage	16	EA	\$ 500	\$ 8,000	
Subtotal				\$ 2,114,200	\$ 2,114,200
Contractor Mobilization			5%	\$ 105,700	
Traffic Control and Signage/Message Boards			4%	\$ 85,100	
Water Pollution Control and SWPPP			1%	\$ 21,100	
Hard Cost Construction Subtotal				\$ 2,326,100	
10% Contingency			10%	\$ 232,600	
Construction Inspection (15% of Construction)			15%	\$ 348,900	
Construction Subtotal				\$ 2,907,600	
Administration: Planning, Coordination, Cost Recovery			15%	\$ 348,900	
Design including Survey, Plans, Specifications			12%	\$ 279,100	
Project Budget				\$ 3,535,600	



4/8/2011

PEAK HOUR SIGNAL WARRANT ANALYSIS (2003 MUTCD)

PROJECT NAME: **CONCORD OSIP UPDATE 2010**

SCENARIO: **EXISTING**

COMMENTS: **n/a**

MAJOR STREET: **CLAYTON RD** NB/SB EB/WB # OF APPROACH LANES: **4**

MINOR STREET: **SUTTER ST** NB/SB EB/WB # OF APPROACH LANES: **1**

THE STUDY INTERSECTION HAS MORE THAN THREE APPROACHES (Y OR N): **Y**

ISOLATED COMMUNITY WITH POPULATION LESS THAN 10,000 (Y OR N): **N**

85TH PERCENTILE SPEED GREATER THAN 40 MPH ON MAJOR STREET (Y OR N): **N**

WORST CASE DELAY FOR MINOR STREET APPROACH:	AM	12.3	sec	PM	0.00	sec
		0.14	veh-hr		0.00	veh-hr

	MAJOR STREET		Total	MINOR STREET		Heavy Leg	Total	Intersection Total
	EB Approach	WB Approach		NB Approach	SB Approach			
06:00 AM TO 07:00 AM			0			0	0	0
07:00 AM TO 08:00 AM	1,072	1,259	2,331	1	42	42	43	2,374
08:00 AM TO 09:00 AM			0			0	0	0
09:00 AM TO 10:00 AM			0			0	0	0
10:00 AM TO 11:00 AM			0			0	0	0
11:00 AM TO 12:00 PM			0			0	0	0
12:00 PM TO 01:00 PM			0			0	0	0
01:00 PM TO 02:00 PM			0			0	0	0
02:00 PM TO 03:00 PM			0			0	0	0
03:00 PM TO 04:00 PM			0			0	0	0
04:00 PM TO 05:00 PM			0			0	0	0
05:00 PM TO 06:00 PM	1,665	830	2,495	8	197	197	205	2,700
06:00 PM TO 07:00 PM			0			0	0	0
07:00 PM TO 08:00 PM			0			0	0	0
08:00 PM TO 09:00 PM			0			0	0	0
09:00 PM TO 10:00 PM			0			0	0	0

MAJOR STREET		MINOR STREET		INTERSECTION	
	Total	Heavy Leg	Total		Total
AM MAX	2,331	42	43	AM MAX	2,374
PM MAX	2,495	197	205	PM MAX	2,700

Traffic Signal Warrants Worksheet
 Warrant 3: Peak Hour
 Source: MUTCD 2003 California Supplement

Scenario: EXISTING AM
 Intersection: CLAYTON RD AND SUTTER ST
 Comments: n/a

PEAK HOUR WARRANT SATISFIED: NO

PART A

(All parts 1, 2, and 3 below must be satisfied)

SATISFIED **NO**

1. The total delay experienced for traffic on one minor street approach controlled by a STOP sign equals or exceeds four vehicle-hours for a one-lane approach and five vehicle hours for a two-lane approach; AND
2. The volume on the same minor street approach equals or exceeds 100 vph for one moving lane of traffic or 150 vph for two moving lanes; AND
3. The total entering volume serviced during the hour equals or exceeds 800 vph for intersection with four or more approaches or 650 vph for intersection with less than four approaches.

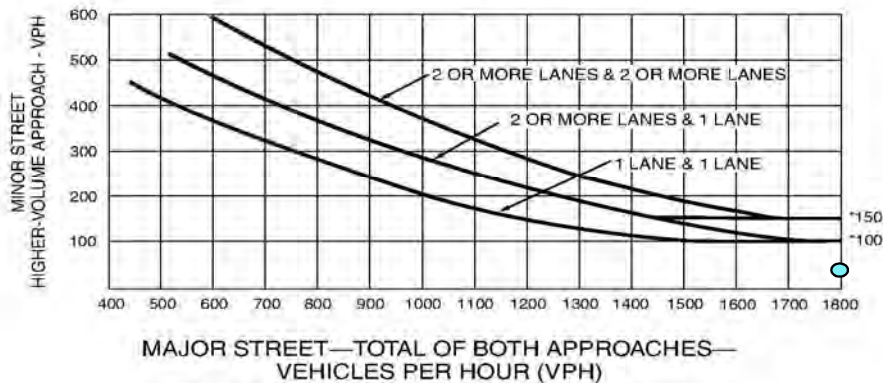
PART B

SATISFIED **NO**

APPROACH LANES	One	2 or More
Both Approaches - Major Street		2331
Highest Approache - Minor Street	42	

The plotted points for vehicles per hour on major streets (both approaches) and the corresponding per hour higher volume minor street approach (one direction only) for one hour (any consecutive 15 minute period) fall above applicable curves in MUTCD Figure 4C-3.

Figure 4C-3. Warrant 3, Peak Hour



*Note: 150 vph applies as the lower threshold volume for a minor-street approach with two or more lanes and 100 vph applies as the lower threshold volume for a minor-street approach with one lane.

Traffic Signal Warrants Worksheet
 Warrant 3: Peak Hour
 Source: MUTCD 2003 California Supplement

Scenario: EXISTING PM
 Intersection: CLAYTON RD AND SUTTER ST
 Comments: n/a

PEAK HOUR WARRANT SATISFIED: YES

PART A

(All parts 1, 2, and 3 below must be satisfied)

SATISFIED **NO**

- 1. The total delay experienced for traffic on one minor street approach controlled by a STOP sign equals or exceeds four vehicle-hours for a one-lane approach and five vehicle hours for a two-lane approach; AND
- 2. The volume on the same minor street approach equals or exceeds 100 vph for one moving lane of traffic or 150 vph for two moving lanes; AND
- 3. The total entering volume serviced during the hour equals or exceeds 800 vph for intersection with four or more approaches or 650 vph for intersection with less than four approaches.

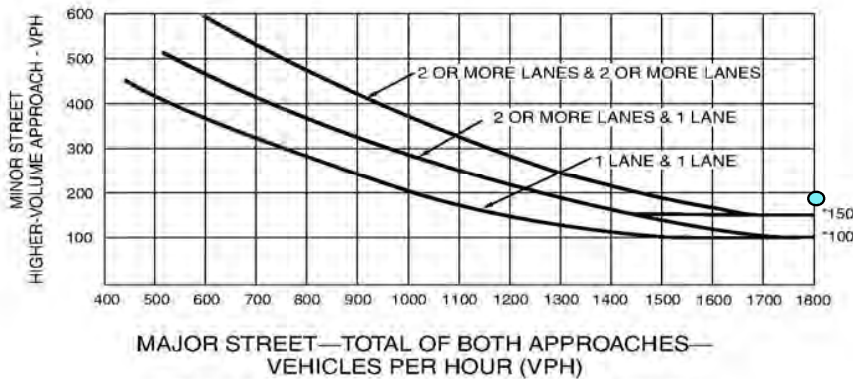
PART B

SATISFIED **YES**

APPROACH LANES	One	2 or More
Both Approaches - Major Street		2495
Highest Approache - Minor Street	197	

The plotted points for vehicles per hour on major streets (both approaches) and the corresponding per hour higher volume minor street approach (one direction only) for one hour (any consecutive 15 minute period) fall above applicable curves in MUTCD Figure 4C-3.

Figure 4C-3. Warrant 3, Peak Hour

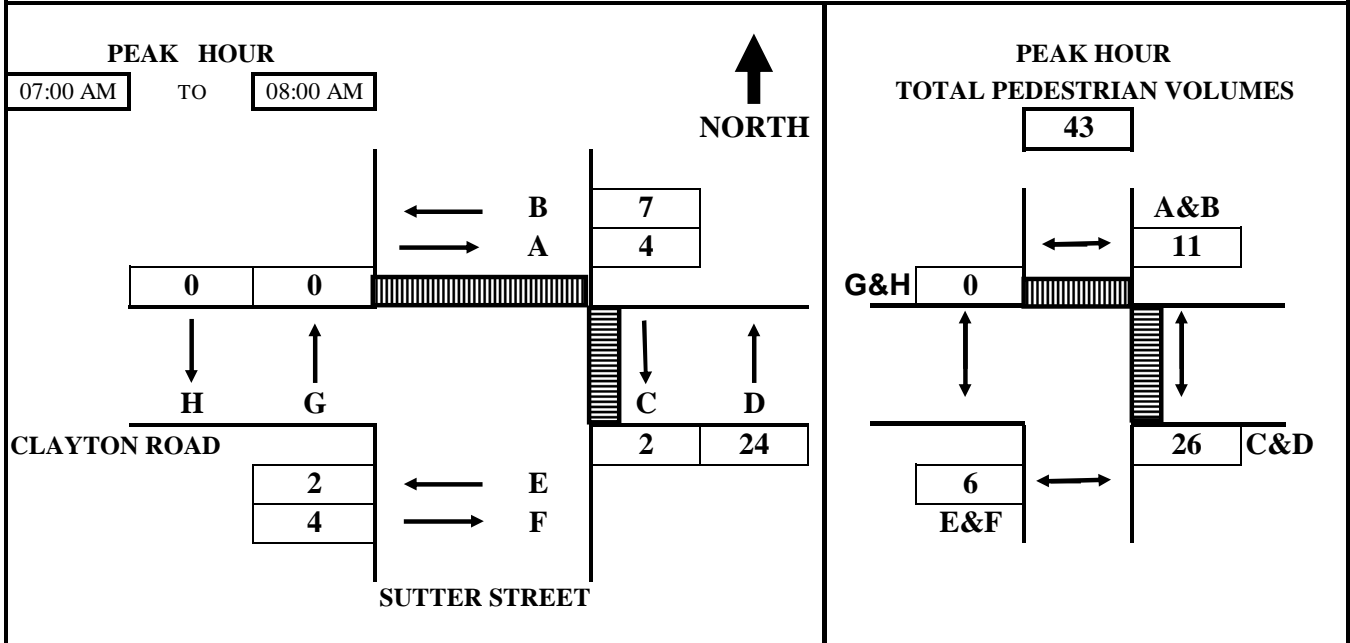


*Note: 150 vph applies as the lower threshold volume for a minor-street approach with two or more lanes and 100 vph applies as the lower threshold volume for a minor-street approach with one lane.

B. A. Y. M. E. T. R. I. C. S.

PEDESTRIAN MOVEMENT SUMMARY

PROJECT: TRAFFIC STUDY IN CONCORD	SURVEY DATE 4/6/2011
N-S APPROACH: SUTTER STREET	DAY: WEDNESDAY
E-W APPROACH: CLAYTON ROAD	CITY: CONCORD
SURVEY PERIOD 7:00 AM TO 9:00 AM	FILE: 3104029-3PED(AM)



TIME PERIOD		NORTH X-WALK		EAST X-WALK		SOUTH X-WALK		WEST X-WALK		TOTAL
From	To	A	B	C	D	E	F	G	H	

SURVEY DATA

07:00 AM	---	07:15 AM	0	1	1	3	0	0	0	0	5
07:15 AM	---	07:30 AM	0	4	1	17	1	0	0	0	23
07:30 AM	---	07:45 AM	4	6	2	24	2	3	0	0	41
07:45 AM	---	08:00 AM	4	7	2	24	2	4	0	0	43
08:00 AM	---	08:15 AM	4	7	2	25	5	4	0	0	47
08:15 AM	---	08:30 AM	5	7	2	29	6	6	0	0	55
08:30 AM	---	08:45 AM	6	7	9	31	8	6	0	0	67
08:45 AM	---	09:00 AM	6	8	10	37	8	8	0	0	77

TOTAL BY PERIOD

07:00 AM	---	07:15 AM	0	1	1	3	0	0	0	0	5
07:15 AM	---	07:30 AM	0	3	0	14	1	0	0	0	18
07:30 AM	---	07:45 AM	4	2	1	7	1	3	0	0	18
07:45 AM	---	08:00 AM	0	1	0	0	0	1	0	0	2
08:00 AM	---	08:15 AM	0	0	0	1	3	0	0	0	4
08:15 AM	---	08:30 AM	1	0	0	4	1	2	0	0	8
08:30 AM	---	08:45 AM	1	0	7	2	2	0	0	0	12
08:45 AM	---	09:00 AM	0	1	1	6	0	2	0	0	10

HOURLY TOTALS

07:00 AM	---	08:00 AM	4	7	2	24	2	4	0	0	43
07:15 AM	---	08:15 AM	4	6	1	22	5	4	0	0	42
07:30 AM	---	08:30 AM	5	3	1	12	5	6	0	0	32
07:45 AM	---	08:45 AM	2	1	7	7	6	3	0	0	26
08:00 AM	---	09:00 AM	2	1	8	13	6	4	0	0	34

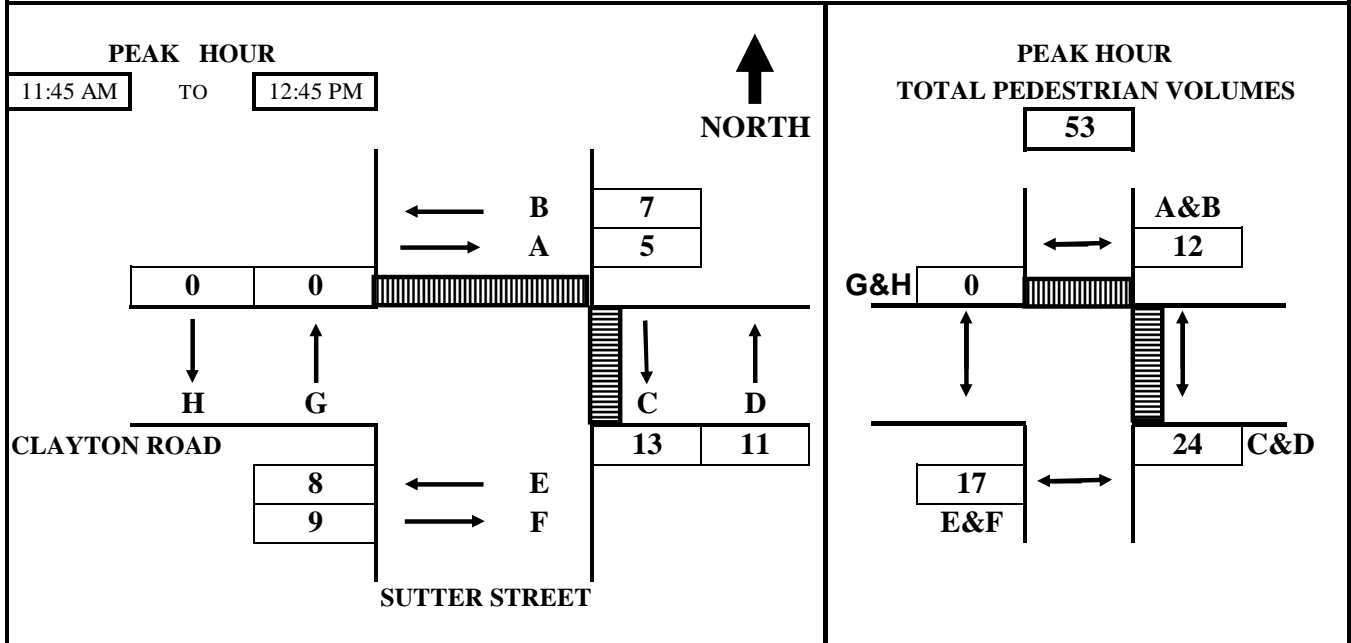
Tel : (510) 232-1271

Fax: (510) 232-1272

B.A.Y.M.E.T.R.I.C.S.

PEDESTRIAN MOVEMENT SUMMARY

PROJECT: TRAFFIC STUDY IN CONCORD	SURVEY DATE 4/6/2011
N-S APPROACH: SUTTER STREET	DAY: WEDNESDAY
E-W APPROACH: CLAYTON ROAD	CITY: CONCORD
SURVEY PERIOD 11:00 AM TO 1:00 PM	FILE: 3104029-3PED(MD)



TIME PERIOD		NORTH X-WALK		EAST X-WALK		SOUTH X-WALK		WEST X-WALK		TOTAL
From	To	A	B	C	D	E	F	G	H	

SURVEY DATA

11:00 AM	---	11:15 AM	0	1	2	2	0	2	0	0	7
11:15 AM	---	11:30 AM	1	1	3	4	1	3	0	0	13
11:30 AM	---	11:45 AM	1	1	5	5	1	6	0	0	19
11:45 AM	---	12:00 PM	2	4	7	6	2	6	0	0	27
12:00 PM	---	12:15 PM	5	7	15	10	2	10	0	0	49
12:15 PM	---	12:30 PM	6	8	17	11	5	10	0	0	57
12:30 PM	---	12:45 PM	6	8	18	16	9	15	0	0	72
12:45 PM	---	01:00 PM	6	9	20	18	9	17	0	0	79

TOTAL BY PERIOD

11:00 AM	---	11:15 AM	0	1	2	2	0	2	0	0	7
11:15 AM	---	11:30 AM	1	0	1	2	1	1	0	0	6
11:30 AM	---	11:45 AM	0	0	2	1	0	3	0	0	6
11:45 AM	---	12:00 PM	1	3	2	1	1	0	0	0	8
12:00 PM	---	12:15 PM	3	3	8	4	0	4	0	0	22
12:15 PM	---	12:30 PM	1	1	2	1	3	0	0	0	8
12:30 PM	---	12:45 PM	0	0	1	5	4	5	0	0	15
12:45 PM	---	01:00 PM	0	1	2	2	0	2	0	0	7

HOURLY TOTALS

11:00 AM	---	12:00 PM	2	4	7	6	2	6	0	0	27
11:15 AM	---	12:15 PM	5	6	13	8	2	8	0	0	42
11:30 AM	---	12:30 PM	5	7	14	7	4	7	0	0	44
11:45 AM	---	12:45 PM	5	7	13	11	8	9	0	0	53
12:00 PM	---	01:00 PM	4	5	13	12	7	11	0	0	52

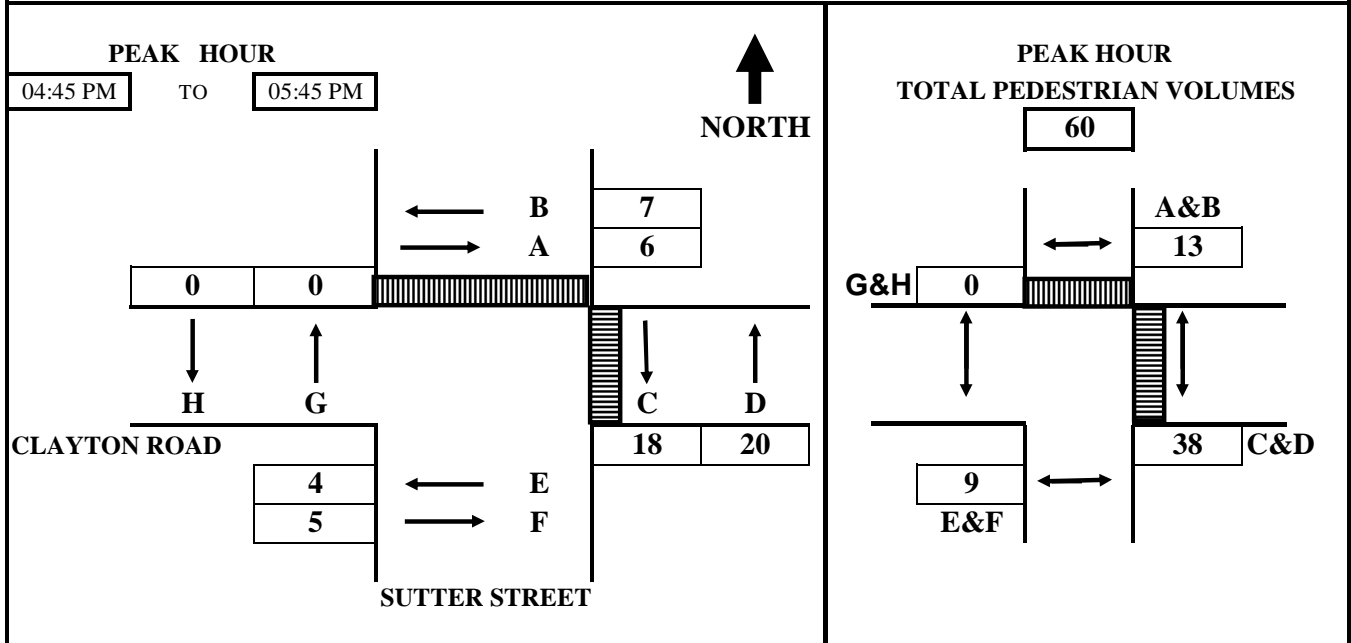
Tel : (510) 232-1271

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B.A.Y.M.E.T.R.I.C.S.

PEDESTRIAN MOVEMENT SUMMARY

PROJECT: TRAFFIC STUDY IN CONCORD	SURVEY DATE: 4/6/2011
N-S APPROACH: SUTTER STREET	DAY: WEDNESDAY
E-W APPROACH: CLAYTON ROAD	CITY: CONCORD
SURVEY PERIOD: 4:00 PM TO 6:00 PM	FILE: 3104029-3PED(PM)



TIME PERIOD		NORTH X-WALK		EAST X-WALK		SOUTH X-WALK		WEST X-WALK		TOTAL
From	To	A	B	C	D	E	F	G	H	

SURVEY DATA

04:00 PM	---	04:15 PM	0	0	1	1	0	3	0	0	5
04:15 PM	---	04:30 PM	0	0	4	3	2	4	0	0	13
04:30 PM	---	04:45 PM	2	3	5	5	3	5	0	0	23
04:45 PM	---	05:00 PM	4	5	11	8	3	5	0	0	36
05:00 PM	---	05:15 PM	8	6	17	15	5	5	0	0	56
05:15 PM	---	05:30 PM	8	7	22	19	5	8	0	0	69
05:30 PM	---	05:45 PM	8	10	23	25	7	10	0	0	83
05:45 PM	---	06:00 PM	8	10	27	27	7	12	0	0	91

TOTAL BY PERIOD

04:00 PM	---	04:15 PM	0	0	1	1	0	3	0	0	5
04:15 PM	---	04:30 PM	0	0	3	2	2	1	0	0	8
04:30 PM	---	04:45 PM	2	3	1	2	1	1	0	0	10
04:45 PM	---	05:00 PM	2	2	6	3	0	0	0	0	13
05:00 PM	---	05:15 PM	4	1	6	7	2	0	0	0	20
05:15 PM	---	05:30 PM	0	1	5	4	0	3	0	0	13
05:30 PM	---	05:45 PM	0	3	1	6	2	2	0	0	14
05:45 PM	---	06:00 PM	0	0	4	2	0	2	0	0	8

HOURLY TOTALS

04:00 PM	---	05:00 PM	4	5	11	8	3	5	0	0	36
04:15 PM	---	05:15 PM	8	6	16	14	5	2	0	0	51
04:30 PM	---	05:30 PM	8	7	18	16	3	4	0	0	56
04:45 PM	---	05:45 PM	6	7	18	20	4	5	0	0	60
05:00 PM	---	06:00 PM	4	5	16	19	4	7	0	0	55

Tel : (510) 232-1271

Fax: (510) 232-1272

EXHIBIT 6

Review Of Reported Collisions

Locations: Signalized and Unsignalized Intersections along Clayton Rd, Galindo St, Concord Blvd and Willow Pass Rd

Date: 11/08/2011

#	Intersection	Type of Control	Period	Total # of Months	Total # of Collisions	# of Collisions (Bike/Ped)	# of Collisions (Injury)	# of Collisions (Fatality)
1	Clayton Rd @ Market St	Traffic Signal	10/01/06 - 09/30/2011	60	45	0	5	1
2	Clayton Rd @ Pine St	Traffic Signal	10/01/06 - 09/30/2011	60	13	1 (Ped)	3	0
3	Clayton Rd @ Detroit Ave	Traffic Signal	10/01/06 - 09/30/2011	60	35	1 (Ped) & 2 (Bike)	10	0
4	Clayton Rd @ Gateway Bl	Traffic Signal	10/01/06 - 09/30/2011	60	26	0	9	0
5	Clayton Rd @ Adelaide St	Only Adelaide St is Stop Controlled	10/01/06 - 09/30/2011	60	4	1 (Bike)	1	0
6	Clayton Rd @ Fry Way	Traffic Signal	10/01/06 - 09/30/2011	60	37	1 (Ped) & 2 (Bike)	11	0
7	Clayton Rd @ Ellis St	Traffic Signal	10/01/06 - 09/30/2011	60	23	1 (Ped) & 1 (Bike)	6	0
8	Clayton Rd @ Sutter St	Only Sutter St is Stop Controlled	10/01/06 - 09/30/2011	60	15	2 (Ped) & 1 (Bike)	6	0
9	Clayton Rd @ Ashbury Dr	Only Ashbury St is Stop Controlled	10/01/06 - 09/30/2011	60	5	0	2	0
10	Clayton Rd @ Amador Ave	Only Amador Ave is Stop Controlled	10/01/06 - 09/30/2011	60	3	0	0	0
11	Clayton Rd @ Galindo St	Traffic Signal	10/01/06 - 09/30/2011	60	73	4 (Ped) & 1 (Bike)	28	0
12	Willow Pass Rd @ Market St /Pine St	Traffic Signal	10/01/06 - 09/30/2011	60	77	5 (Bike)	23	0
13	Willow Pass Rd @ Gateway Bl	Traffic Signal	10/01/06 - 09/30/2011	60	8	1 (Bike)	2	1
14	Willow Pass Rd @ Fry Way	Traffic Signal	10/01/06 - 09/30/2011	60	42	4 (Bike)	14	0
15	Willow Pass Rd @ Sutter St	Traffic Signal	10/01/06 - 09/30/2011	60	10	1 (Ped)	4	0
16	Willow Pass Rd @ Mira Vista Tr	Only Mira Vista Tr is Stop Controlled	10/01/06 - 09/30/2011	60	9	2 (Bike)	3	0
17	Willow Pass Rd @ Galindo St	Traffic Signal	10/01/06 - 09/30/2011	60	95	3 (Ped) & 2 (Bike)	20	1
18	Concord Blvd @ Galindo St	Traffic Signal	10/01/06 - 09/30/2011	60	82	1 (Ped)	29	0
				602	37	176	3	

#	Mid-Block	Limit	Period	Total # of Months	Total # of Collisions	# of Collisions (Bike/Ped)	# of Collisions (Injury)	# of Collisions (Fatality)
1	Clayton Rd	Ashbury Dr - Amador Ave	10/01/06 - 09/30/2011	60	1	0	1	0
2	Concord Blvd	Mira Vista Tr - Galindo St	10/01/06 - 09/30/2011	60	2	0	0	0
				3	0	1	0	

November 14, 2011

Danea Gemmell
City Engineer
City of Concord
1950 Parkside Dr. M/S 40
Concord, CA 94519

Subject: CC-TLC Measure J Transportation for Livable Communities
City of Concord
Central Concord Pedestrian Improvements & Streetscape Project

Dear Ms. Gemmell:

The Todos Santos Business Association (TSBA) is happy to support the City of Concord's 2011-12 CC-TLC Measure J Transportation for Livable Communities project application. Of particular interest to the TSBA is Concord's plan for Downtown Streetscape Improvements.

As a part of the downtown for many years now, the TSBA is excited at the possibility of additional improvements implemented in the downtown and the Todos Santos Plaza neighborhood. Construction of Concord's project supports the TSBA's efforts to continue to provide safe pedestrian and bicycle-friendly options for our customers and employees, alike. One of the numerous benefits to running and managing a business in Concord is the multitude of commuting options available through two BART stations and several other local and regional transit options. By improving the accessibility in this area through additional wayfinder kiosks, installation of bike routes and associated storage facilities, and increased safety through additional lighting, the TSBA members are able to continue to encourage their employees and customers to visit many of the restaurants, shops and services available in our walkable neighborhood.

Over the last few years, the City of Concord has made a concentrated effort to redevelop the downtown area, specifically around Todos Santos Plaza, and it has completely revitalized the entire community. By continuing these efforts through this grant, the City would create additional connections throughout Central Concord that support pedestrian and bicycle-friendly accommodations, which would further expand the business community's opportunities for success.

Again, the TSBA is proud to support and encourage the City of Concord's 2011-12 CC-TLC Measure J Transportation for Livable Communities project application and looks forward to providing assistance wherever possible.

Sincerely,



Virginia Thomas

President

Todos Santos Business Association

November 11, 2011



Danea Gemmell, PE
City Engineer
City of Concord
1950 Parkside Drive
Concord, CA 94519-2578

RE: WILLOW PASS ROAD BEAUTIFICATION INITIATIVE

Dear Ms. Gemmell;

Harris & Associates has been a proud member of the City of Concord business community for 23 years. Our corporate office moved to Willow Pass Road in 2008 from near Port Chicago Highway, and we are very pleased with the location and proximity to the vibrant downtown. With a staff of over 120 employees we appreciate the many amenities nearby and ease of walking to restaurants and retail stores.

However, as good a location as it is, it would be a real plus to have streetscape improvements to the sidewalks and roadway crossings in our area. Decorative sidewalks, enhanced pedestrian crosswalks, street trees and furniture similar to the downtown core area around Todos Santos Plaza, would make a major impact in the beautification of Willow Pass Road as it extends from the civic center to north of SR 242.

It has come to our attention that the City may have the opportunity to pursue funding for these types of upgrades and we would like to lend our support and encouragement to realize this effort. One can easily see the effect of the improvements around the downtown core area and we think similar beautification efforts in our area would have a very positive effect on business and the public.

Sincerely,
HARRIS & ASSOCIATES

A handwritten signature in blue ink, appearing to read "Vern Phillips".

Vern Phillips, PE
Senior Vice President

November 14, 2011



Danea Gemmell
City Engineer
City of Concord
1950 Parkside Dr. M/S 40
Concord, CA 94519

JCM Partners, LLC
2151 Salvio Street
Suite 325
Concord, California
94520

Mailing Address:
P.O. Box 3000
Concord, CA
94522-3000

Tel: 925.676.1966
Fax: 925.676.1744
www.rent-one.com
www.jcmpartners.com

Subject: CC-TLC Measure J Transportation for Livable Communities
City of Concord
Central Concord Pedestrian Improvements & Streetscape Project

Dear Ms. Gemmell:

The Todos Santos Business Association (TSBA) and JCM Partners are happy to support the City of Concord's 2011-12 CC-TLC Measure J Transportation for Livable Communities project application. Of particular interest to the TSBA and JCM Partners is Concord's plan for Downtown Streetscape Improvements.

As a part of the downtown for many years now, the TSBA and JCM Partners are excited at the possibility of additional improvements implemented in the downtown and the Todos Santos Plaza neighborhood. Construction of Concord's project supports the TSBA's efforts to continue to provide safe pedestrian and bicycle-friendly options for our customers and employees, alike. One of the numerous benefits to running and managing a business in Concord is the multitude of commuting options available through two BART stations and several other local and regional transit options. By improving the accessibility in this area through additional wayfinder kiosks, installation of bike routes and associated storage facilities, and increased safety through additional lighting, the TSBA members are able to continue to encourage their employees and customers to visit many of the restaurants, shops and services available in our walkable neighborhood.

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Again, the TSBA and JCM Partners are proud to support and encourage the City of Concord's 2011-12 CC-TLC Measure J Transportation for Livable Communities project application and looks forward to providing assistance wherever possible.

Sincerely,
JCM Partners, LLC

A handwritten signature in blue ink that reads "Jeffrey K. Woods".

Jeffrey K. Woods
Property Manager and Member
Todos Santos Business Association



November 14, 2011

Danea Gemmell
City Engineer
City of Concord
1950 Parkside Dr. M/S 40
Concord, CA 94519

Subject: CC-TLC Measure J Transportation for Livable Communities
City of Concord
Central Concord Pedestrian Improvements & Streetscape Project

Dear Ms. Gemmell:

The Todos Santos Business Association (TSBA) is happy to support the City of Concord's 2011-12 CC-TLC Measure J Transportation for Livable Communities project application. Of particular interest to the TSBA is Concord's plan for Downtown Streetscape Improvements.

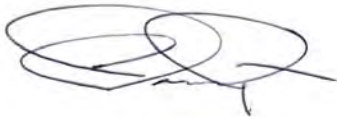
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3569 MT. DIABLO BLVD., SUITE 200, LAFAYETTE, CA 94549
TELEPHONE 925 299 8170 • FAX 925 284 2331
www.MSPSinc.COM

Again, the TSBA is proud to support and encourage the City of Concord's 2011-12 CC-TLC Measure J Transportation for Livable Communities project application and looks forward to providing assistance wherever possible.

Cordially,
Main Street Property Services, Inc.

A handwritten signature in black ink, appearing to read 'Craig Semmelmeier', with a stylized flourish at the end.

Craig Semmelmeier
Principal
License #01087433

Review from Bora C.

50 friends

91 reviews

Lafayette, CA**3/10/2011** 24 Check-ins Here

Despite the minimum parking, this shopping center has **decent stores for being so old**. My regular spots are Parkway Cleaners for my dry cleaning, Las Montanas (the actual restaurant or the market) The Mediterranean Korea House Mimi's for lunch, Sunrise Bagels for bfast, 99 cent store, Choice to pick up some t-shirts, Jo-Ann's fabrics for arts and craft items, and my optometrist's office **all in one parking shop!**

Review from Terry T.

23 friends

347 reviews

Hercules, CA**11/5/2010**

Park N' Shop is not a typical strip mall. It is chock-full of restaurants, and only 4 of the 22 are chains. Add to this bounty: cleaners, tailors, a book store and lots of small stores. There is also more than one ethnic market. **I find Park N' Shop to be a vibrant strip mall and truly appreciate that it is older and does not have a cookie cutter look** and feel that most of the newer ones do.

Review from Paul M.

38 friends

76 reviews

Bay Area**11/17/2009**

In spite of all of the 1 star reviews, you are probably wondering why I am giving it four stars. Easy.

You have to take Park 'N' Shop for what it is...and not whine about what it is not...

Park 'N' Shop has many cheap, good restaurants. Namaste, The Mediterranean, Korea House, Saigon Bistro, several taquerias...all real good food.

Sure, it is a total hood and a dump in general. So what. As long as Namaste, et al, are there, I'll also be there :)

Review from G G.

1 friend

48 reviews

Walnut Creek, CA**6/3/2008**

I go to Park Place Cleaners here. Also in the same building block are an Office Max, Starbucks and 99-cent store. In walking distance are a Jo-Anns, Burlington Coat Factory and Fry's. There are some other misc stores, including a Mexican grocery market -- got some potatoes there one day and practiced my Spanish. And to round it out are a McDs and Burger King. Who is trying to hang out at Park N Shop? Not me. I don't think I'd try any of the restaurants here. I'm there for 15 min to drop something off or pick something up and I'm out. The 4-way stop can be real stupid, but overall it's easy to get in and out of here.

Yelp Reviews about Park and Shop

Review from Casey D.

51 friends

21 reviews

Laguna Beach, CA

12/5/2007

Despite having a few decent restaurants (Namaste, The Mediterranean, Mimi's Cafe), Park 'N' Shop is run down and naaa-aaasty. With all the renovations being done to downtown Concord, I'm really hoping Park 'N' Shop will get some attention. The worse thing about this strip mall? The traffic flow. I have been so close to getting into accidents because people don't observe the 4-way stops. Beware!

Review from Louise T.

24 friends

23 reviews

Concord, CA

5/17/2011 6 Check-ins Here

My favorite park! We are here 2-3x a week for Tuesday Farmer's Market, ice cream treat, or enjoying the Thursday night market.

The park is clean and well maintained. People are friendly. No bad crowd or people harassing you even late nights. I enjoy walking around with the kids after ice cream.

The 5th star is reserved when they welcome bicycles and put more racks. People should take their bikes to the market, ice cream, or coffee. Concord should encourage this so we can be GREENER, instead of people at the park breathing fumes all the time.

Review from Manda W.

53 friends

11 reviews

Martinez, CA

8/22/2011 10 Check-ins Here

Let me break this place down for you. Yes the shops around the park are fine. You have a hidden gem of a bookstore where you can find a lot of great books for cheap. There is a Baskin Robbins that offers \$1.50 scoops of ice cream, which the kids and adults all love.

The movie theater is right across the street so you can grab dinner here and go to the movies, so 5 stars for the perfect date night setting. Wells Fargo and Bank of America are both located here. Another 5 stars for the farmers market they have on Tuesdays and Thursdays.

Here is where I cut them down two stars though.. The park there sucks. Maybe it's not the playground itself so much, because my kids know how to climb and slide, but it's definitely made for older children in my opinion. My two toddlers get trampled and pushed all over by the crazy older kids when we try to come down and enjoy the farmers market. Alright people, maybe lets just teach the children these days some manners and this wouldn't be an issue.

There is enough area there to maybe add a toddler park, but I guess with our economy these days that won't happen.

Be sure to check out the farmers market though. Lots of local produce for good prices.

Review from Laura F.

22 friends

32 reviews

Martinez, CA

6/2/2011 11 Check-ins Here

Decent during the day, can get kinda sketchy by night.

Pretty decent selection of restaurants, though, and a pretty good Farmer's Market during the week.

Review from Eric B.

1841 friends
3384 reviews

Albany, CA
1/19/2011

Concord was founded under the name of Todos Santos ("All Saints"), a name still borne by this Center City plaza and park, as dedicated by Salvio Pacheco in 1869. This City (also briefly called Drunken Indian at one odd historical point) also achieved prominence in the 19th century when most residents of Pacheco relocated to Concord to avoid the devastation of fire and flood which crippled that Town's formerly booming economy.

Todos Santos Plaza encompasses an entire city block and is known for its farmers market, summer concerts, a large number of surrounding restaurants and shops, and a comfortable green space to hang out and people watch. Much of the area immediately around downtown has been redeveloped, with new high-density apartment and condominium projects to take advantage of the close proximity to public transportation and the "vibrant" area surrounding the park. Despite this, some crime-related issues remain that still makes me cautious.

One other thing that I remember about the Downtown Concord area were the "Spirit Poles" that were installed in 1989. 91 aluminum rods were erected in a median strip along Concord Avenue by artist Gary Rieveschl of Cincinnati. They became unstable after a few years and were destroyed in 2001. The Spirit poles were part of a large scale public art project that extended down Concord Ave from Market St to Salvio St. It was intended as a "gateway" project into the Todos Santos Plaza area. The concept was to instill a feeling of perpetual motion (up/down & side to side), as you drove along Concord Avenue. This was supposed to represent the continual changes of this area from the local indigenous tribes, to the Spanish settlers, and onward to modern day Concord. So could it be that the lost mojo of former Spirit Poles is keeping me from coming out to Todos Santos Plaza more often?

Review from Mona C.

47 friends
59 reviews

Berkeley, CA
8/4/2011

Love the Farmer's Market every Thursday! Live music, food stalls, fresh affordable produce! I got half a flat of beautiful, ripe strawberries for \$6 and yummy peaches for \$1/pound! It's fun to bring a blanket and hang out on the grass. The hot wing stall makes yummy spiral fries that are cut using a power drill! Great place for families and to people watch!

Review from Andre C.

8 friends
128 reviews

Soda Springs, CA
4/26/2011

Nice plaza, small, but plenty of room to just sit in the grass and hang.

Plenty of places to grab food and drinks around. Get something to go and enjoy your food outside!

If I lived in the area, I would be here frequently!

On a sunny day... this place is perfect.

Review from Kristin E.

8 friends

145 reviews

Sacramento, CA

6/2/2011

I think this is a great little plaza with tons of places to eat, shop, drink coffee or watch a movie! Lots to do here and they have a great little farmers market.

Review from Shanna S.

7 friends

7 reviews

Concord, CA

7/2/2011

I came here for the first time with co workers and my daughter on a Thursday, the atmosphere was great. My daughter was overwhelmed by the crowd and she didn't want to play on the playground because it was again very crowded. We walked around once and then she was ready to go. I enjoyed the environment and will come back on another Thursday for the farmers market. Since it was Thursday the parking was not great, but what do you expect for a popular event.

Review from Jules S.

523 friends

313 reviews

Haleiwa, HI

5/10/2011 1 Check-in Here

It's a pretty small park, but the surrounding area has a lot to offer. The cool thing was I got to see a cover band for Johnny Cash on Sunday. The singer sounded just like Johnny. He definitely dressed like Johnny Cash! I guess they have live music here every so often to entertain the citizens of Concord. That's cool! The cover band was good. The park was OK. The audience was pretty boring though. The band was good. Show some love people!

Review from Serra N.

96 friends

293 reviews

Martinez, CA

1/11/2011 16 Check-ins Here

I grew up playing at Todos Santos park as a child and now my daughter plays here. We used to go to the ice cream counter at Rexall Drugs, I saw Alice in Wonderland at the movie theater before it changed into the Pussycat theater. The surrounding plaza has a

Yelp Reviews about Downtown Area-Todos Santos Plaza

variety of places to eat ranging from casual to elegant dining. I am looking forward to spending many lazy summer afternoons here with my daughter.

Review from Nikki P.

8 reviews

Walnut Creek, CA

3/2/2011

Love their farmer's market Tuesdays! And during the summer there is also an additional Thursday night market complete with live music in the park. Having a town square is a unique and special characteristic that Concord is lucky to have. Favorite Plaza locations include: Peet's Coffee - the staff is always friendly and the coffee is always freshly brewed and hot! And don't miss Half Price Books - Great treasures and values to be found!! I feel in love with them on assignment in Austin, TX and was delighted to come upon them right here in Concord! Great for the bookworm on your gift list!

Review from Rob W.

2 reviews

Concord, CA

2/2/2011

When I first moved here four years ago, this park was adorable and a really wonderful place to walk around, take in some live music in the summer and grab a coffee near the beautiful outdoor fountains.

Lately though? Wow. Sketch-o-rama. Especially after dark, the area starts to get taken over by rough necks, crazy homeless people, tweakers and rowdy crowds of teenagers. The last three times I went down there to grab a coffee with someone and chat we've had to leave because there were too many sketchy people actively hassling us. You can only get hit up for spare change or a cigarette so many times before you just want to get the heck out of there!

It used to be that there were a lot more cops patrolling the area and maybe that's what kept the rougher elements at bay, but now it's kind of unavoidable. It's sad, because I love the plaza and the little shops around it are so cute and friendly. But I just don't like having to rush around and keep one eye out at all times for incoming crazy, it totally takes the fun out of things.



Application Form

Measure J Transportation for Livable Communities (CC-TLC) Program Funds

1. PROJECT INFORMATION

Project Name Walnut Creek Pedestrian Master Plan

Project Location Citywide

Sponsor..... City of Walnut Creek

Implementing
 agency.....

Partner agencies.....

Contact for project... Jeremy W. Lochirco, Senior Planner
 City of Walnut Creek, CA
 1666 North Main Street
 Walnut Creek, CA 94596

2. FUNDING

In thousands of dollars, please provide the estimated cost of the proposed project, the amount of CC-TLC funds requested, and any other funds that have been committed to the project.

<i>Source</i>	<i>Amount</i>
Total project cost †	\$120,000
Requested CC-TLC funds	\$100,000
Other committed funding 1 †	\$20,000
Other committed funding 2 ‡	\$000
Unfunded balance	\$000

† Minimum request is \$50,000

‡ Add other committed funding sources as needed

3. POTENTIAL PHASING

It is anticipated that the Pedestrian Master Plan would be adopted in 2013. Plan implementation would start in 2013.

4. PROJECT DESCRIPTION

The City of Walnut Creek is committed to walking as a form of transportation and recreation that is safe, accessible, healthy, and affordable for all citizens. The Pedestrian Master Plan will help Walnut Creek accomplish several goals of the City's General Plan 2025 and the Transportation for Livable Communities program that will provide a framework of policies, actions and goals to make walking safer, easier, and more attractive to residents and visitors; provide design standards for physical improvements related to the overall pedestrian experience; and outline steps to encourage walking as a healthy alternative to automobile travel.

Specifically, the Walnut Creek Pedestrian Master Plan would:

1. Present a vision for Walnut Creek's pedestrian network, based on a set of goals, policies and actions from the Walnut Creek General Plan 2025.
2. Identify and discuss the various local, regional and other planning and policy documents that relate to the implementation of pedestrian facilities.
3. Identify the existing pedestrian network throughout the City and evaluate the overall pedestrian infrastructure, including impediments which limit pedestrian access and mobility.
4. Identify current and future pedestrian travel and incorporate specific recommendations from the City of Walnut Creek's Pedestrian Safety Audit.
5. Recommended short and long range capital projects to improve mobility, access and safety.
6. Recommended programs to educate, encourage and increase walkability and pedestrian awareness.
7. Demonstrate compliance with the Americans with Disabilities Act and recommend improvements to provide routine accommodations.
8. Develop and adopt Complete Street policies.
9. Recommend possible revisions to the Walnut Creek Zoning Ordinance and Design Review Guidelines to provide more walkable and pedestrian-friendly development throughout the City.

10. Discuss implementation and funding for projects and programs in the Pedestrian Master Plan.

5. PROPOSED SCHEDULE

In the following table, enter the expected beginning and ending *month and year* for each of the phases listed. If the phase has been finished, enter "Completed" in the "To" column.

<i>Phase</i>	<i>From</i>	<i>To</i>
Preliminary Design & Planning (Plan)	Spring 2012	Fall 2012
Design	N/A	N/A
Environmental Clearance	Fall 2012	Spring 2013
Right-of-Way	N/A	N/A
Construction	N/A	N/A

6. ABILITY TO MEET CRITERIA

Describe how the proposed project would meet the following four criteria:

Achievement of CC-TLC Goals:
Identify which of the six goals of the CC-TLC program the proposed project would help achieve — a project need not meet all of them — and *briefly* describe how it would do so (see Exhibit One for these six goals)

Help create walkable, pedestrian friendly neighborhoods and business districts and create pedestrian-friendly access linking housing to job centers and transit.

The development of a Pedestrian Master Plan would help to provide policies and guidelines for facilities that will make walking safer, easier and more attractive to residents and visitors; provide design standards for physical improvements related to the pedestrian experience; outline steps to encourage walking as a healthy alternative to automobile travel; and provide recommended Complete Streets policies.

In Walnut Creek, both pedestrians and public transportation are essential for the economic health and vitality of downtown Walnut Creek, where space is limited for expanded parking. The Pedestrian Master Plan will create policies recognizing pedestrian, bicyclists and public transportation needs to accommodate residents and visitors who come to Walnut Creek.

Promote innovating solutions, including compact building design and context sensitive planning that is integrated with the transportation system.

City sidewalks are important social spaces as well as travel routes. As a result, standards for sidewalk widths must be developed to calculate optimal sidewalk widths, especially in the City's commercial areas. The Pedestrian Master Plan will be developed, incorporating context sensitive planning solutions for physically sensitive areas. Just as building context is an essential component along a streetscape to create a pedestrian friendly environment, sidewalk width is equally as important. Having too much sidewalk space is just as undesirable as having too little as too much width makes a sidewalk seem 'empty'. The Walnut Creek Pedestrian Master Plan will examine the existing sidewalk conditions, along with building context, and propose innovative solutions to improve the overall pedestrian experience.

Help create affordable housing.

While the project will not directly create affordable housing, the Pedestrian Plan will promote an environment that supports the residents of those developments. Most transportation studies suggest that car ownership is lower with affordable housing than market rate housing. As a result, residents within affordable housing developments will more heavily rely on public transportation and walking as a means to reach their destination. The Pedestrian Master Plan will support the development of affordable housing in the City and in particular, the Golden Triangle area bounded by Highway 680, Parkside Drive, Main Street, California Boulevard and Riviera Avenue. This area was selected due to the proximity to the Walnut Creek BART Station and walkable location near downtown.

Encourage a mixture of land uses and support a community's development or redevelopment activities.

Unlike many other East Bay jurisdictions, Walnut Creek is a large regional office and shopping destination employing over 53,000 persons throughout the City. In order for the City to provide a walkable, pedestrian friendly environment, Walnut Creek needs to develop a strategic planning document to facilitate short and long range pedestrian improvements. As the City continues to redevelop, the Pedestrian Master Plan will help Walnut Creek identify opportunities necessary to create a more sustainable development.

Provide for a variety of transportation choices to enhance a community's mobility, identity, and quality of life.

While walking is the least expensive transportation mode, building and maintaining a high quality pedestrian infrastructure requires comprehensive planning and long term funding. The Pedestrian Master Plan, with the inclusion of Complete Streets policies, will establish goals, policies and action items that incorporate and accommodate all transportation modes (Public Transportation, Automobile, Bicycle and Pedestrian).

Feasibility: Describe why the project is feasible and any outstanding issues in the project development process — design, environmental clearance, right-of-way purchase, and PS&E — or funding of the project

The Walnut Creek Pedestrian Master Plan is envisioned as the first step – and an essential component – to securing additional local, regional and federal grant monies that improve pedestrian safety and accessibility throughout the City. The Plan will be developed to meet or exceed all local, state and federal guidelines and no issues for Plan development are currently anticipated.

Local and policy support: Identify local policies that support the project and the integration of the project with other local efforts, as well as other support from the general public, the RTPCs and other relevant agencies

Walnut Creek General Plan 2025

Goal 6 of the City's Transportation Element states that the City shall provide a safe and attractive walking environment accessible to all.

Action 6.2.4 recommends that the City require appropriate pedestrian improvements as a condition of site development, design review, subdivision, or building permit approval and for all City street-widening projects.

Policy 3 of the City's General Plan Housing Element states that the City should encourage housing and commercial mixed-use development in selected locations that enhances pedestrian access and reduces traffic, particularly in the Core Area, and near public transit.

Walnut Creek Bicycle Plan

Action 8.3 indicates the City shall provide routine accommodations for all modes of travel when implementing City street-widening projects and encourage routine accommodations for all other street improvement projects, where possible

Action 9.4 recommends the City develop a Complete Streets policy that includes vehicle, pedestrian and bicycle related improvements, for upgraded roadways in Walnut Creek.

Action 10.1 recommends the City enhance the bicycle and pedestrian network connectivity between transit stops and major destinations, including the Core Area and the City's open space areas.

Countywide Bicycle and Pedestrian Plan

Goal 1 Expand, Improve and Maintain facilities for walking and bicycling.

Policy 1.3 Provide funding for the construction and maintenance of priority bicycle and pedestrian facilities to provide access to activity centers and other key destinations; connect cities within Contra Costa; and connect Contra Costa to neighboring counties.

Policy 2.1 Give relative funding priority to projects that address safety deficiencies for pedestrians and bicyclists, especially conflicts with motor vehicles.

Policy 2.3 Assist local jurisdictions in planning and designing safe streets by providing information, tools and other resources.

Policy 2.5 Support programs that educate drivers, bicyclists, and pedestrians of their rights and responsibilities, as well as pedestrian and bicycle education and safety programs for adults and youth.

Letter of Support Provided By:

Walnut Creek Bicycle Advisory Committee

Matching funds: Identify any funds from other sources that are or would be committed to the project

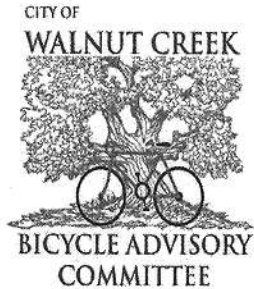
City of Walnut Creek public match – 16%

EXHIBIT ONE

Goals of the Measure J CC-TLC Program

Measure J establishes six goals for the Measure J Transportation for Livable Communities program.

- Help create walkable, pedestrian-friendly neighborhoods and business districts
- Promote innovative solutions, including compact building design and context-sensitive site planning that is integrated with the transportation system
- Help create walkable, pedestrian-friendly access linking housing and job centers to transit
- Help create affordable housing
- Encourage a mixture of land uses and support a community's development or redevelopment activities
- Provide for a variety of transportation choices to enhance a community's mobility, identity, and quality of life



November 10, 2011

Mr. Brad Beck
Senior Transportation Planner
Contra Costa Transportation Authority
2999 Oak Road, Suite 100
Walnut Creek CA 94597

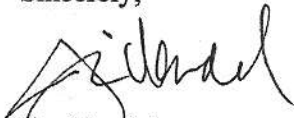
Dear Mr. Beck,

The Walnut Creek Bicycle Advisory Committee (BAC) fully supports the City of Walnut Creek's Transportation for Livable Communities (TLC) grant application for the completion of a Pedestrian and Complete Streets Master Plan for Walnut Creek.

Since its inception, the Walnut Creek BAC has promoted the construction of new pedestrian and bicycle facilities throughout Walnut Creek. The BAC is committed to the growing demand for safe pedestrian and bicycle transportation in Walnut Creek. As you know, fuel prices will continue to rise or remain unpredictable, and people continue to look for alternatives to the automobile. We also believe that a Pedestrian Master Plan will address concerns about the growing obesity epidemic, improve air quality and contribute to an enhanced quality of life.

In 2011, the City of Walnut Creek adopted its Bicycle Master Plan to establish a framework and support future development of bicycle facilities. The BAC believes a Pedestrian Master Plan with Complete Street policies will establish a similar framework for development and encourage the routine accommodation of all transit modes. The Committee would like to express its support of Walnut Creek's TLC application for the creation of a Pedestrian Master Plan and recommends Contra Costa Transportation Authority approve grant funding.

Sincerely,



Jim Vendel
Chairperson
Walnut Creek Bicycle Advisory Committee

2.1 MAINTENANCE

All bicycle facilities should be maintained to help minimize safety issues, including the repair of potholes and other street surface problems, such as the replacement of problematic roadway surfaces and grates.

Goal: Maintain a comprehensive and coordinated bicycle network.

Policy 1 Maintain roadways and bicycle facilities to provide safe conditions for all bicycle users.

- Action 1.1 Keep bicycle facilities maintained and free of debris.
- Action 1.2 Minimize disruption for cyclists during construction of roadway improvements and provide alternate bicycle routes, if possible, to maintain safety and route connectivity.
- Action 1.3 Utilize the Bicycle Advisory Committee and Transportation Commission to help identify locations that pose safety issues for all users.
- Action 1.4 As part of a CIP or roadway improvement, continue to improve signalized intersections, such as the installation of Bike Thru Lanes or type "D" bicycle loop detectors, to accommodate bicyclist use when such improvements are located on a designated bicycle facility.
- Action 1.5 Pursue new grant funding sources for constructing, upgrading, or maintaining new or improved bicycle facilities.
- Action 1.6 Regularly evaluate bicycle-related crashes on roadways and implement short and long term improvements that reduce bicycle crash rates.

Action 1.4

As part of a CIP or roadway improvement, continue to improve signalized intersections, such as the installation of Bike Thru Lanes or type "D" bicycle loop detectors, to accommodate bicyclist use when such improvements are located on a designated bicycle facility.

Policy 2 Update the Bicycle Plan to reflect new or improved bicycle facilities.

- Action 2.1 Regularly review and update the Bicycle Plan to maximize the City's potential to secure grant funding from outside sources for new or improved bicycle facilities.
- Action 2.2 Regularly update and distribute a map that identifies new or expanded bicycle facilities.
- Action 2.3 When preparing the 10-year Capital Investment Program list, incorporate new or improved bicycle facilities that enhance connectivity and improve public safety.

2.2 EDUCATION, ENFORCEMENT & DESIGN

Bicycle education is an essential component in developing safe cycling skills, teaching cyclists their rights and responsibilities, and teaching both motorists and cyclists how to safely and effectively share the road.

Goal: Enhance bicycle safety through education and safety programs aimed at reducing bicycle crashes and eliminating barriers to bicycle use.

Policy 3 Expand education and interaction opportunities between the City and the general public on bicycle related issues.

Action 3.1 Regularly publish articles on bicycle safety and education in the City's *Nutshell* newsletter, on Walnut Creek public television, and on the City's website.

Action 3.2 Provide an annual report to the Transportation Commission and City Council on plan accomplishments and identify future opportunities for plan implementation.

Action 3.3 Support educational programs that promote safe riding techniques through schools, employment centers, and general publicity efforts.

Action 3.4 Coordinate with outside agencies to develop and distribute information that promote bicycle safety and alternative commute modes.

Policy 4 Enforce the safe use sharing of roadways.

Action 4.1 Monitor and enforce the safe sharing of roadways, as necessary.

Goal: Enhance bicycle security through education, regulation, and enforcement.

Policy 5 Promote programs that increase public safety awareness and reduce incidents of bicycle theft.

Action 5.1 Coordinate with 511 Contra Costa, BART, and County Connection to provide ample secure bicycle racks and lockers at transit stations and major destinations.

Action 5.2 Work with regional agencies to promote the creation of a bicycle registration and anti-theft program to help deter bicycle theft.

Action 5.3 Encourage and promote bicycle safety to various users, including school-aged children and adults, to reduce the total number of bicycle crashes and eliminate physical and emotional barriers to bicycle use.



- Action 5.4 Work with law enforcement personnel to promote the safe and legal operation of bicycles throughout the City, including expanded awareness of bicycle laws.

2.3 PROMOTION

Promotion and facilitation of cycling includes providing a comprehensive bikeway network, end-of-trip facilities, and bicycle-transit services, holding events that encourage bicycle use, providing incentives for increased bicycle usage, signage and providing maps with recommended routes.

Goal: Facilitate bicycle use as a healthy alternative to get to work, school, shopping, recreational facilities, and transit stops.

Policy 6 Promote the usage of bicycle facilities and support bicycle-related programs that meet the needs of people of all ages, physical abilities, and economic means.

- Action 6.1 Work with local school districts, 511 Contra Costa, and Contra Costa Transportation Authority to encourage local cycling events, such as Bike-To-School and Bike-To-Work events.

Action 6.2 Promote valet bike parking at public events and venues.

Action 6.3 Coordinate improvements with local school districts and transit agencies to provide safe and accessible bicycle facilities that promote Safe Routes to School and Safe Routes to Transit Programs.

Action 6.4 Promote bicycle travel as a viable alternative commute mode.

Policy 7 Create an efficient network of bike facilities that help support bicycle use as a viable mode of transportation.

Action 7.1 Promote bicycling as a commute mode to reduce traffic congestion.

Action 7.2 Support new development and redevelopment that accommodates and promotes bicycle use.

Action 7.3 Encourage businesses to work with 511 Contra Costa to provide an employee incentive program for bicycling to work.

Action 6.3
Coordinate improvements with local school districts and transit agencies to provide safe and accessible bicycle facilities that promote Safe Routes to School and Safe Routes to Transit Programs.

Action 7.4 Expand the existing or create new bicycle facilities with development and redevelopment of employment districts such as the Shadelands Business Center, the downtown Core Area, and around the BART stations.

Action 7.5 Include pedestrian and bicycle audits with traffic studies, if necessary, to measure the effectiveness of existing bicycle facilities and to support the development of new facilities.

2.4 DESIGN

Goal: Provide a safe and attractive environment for bicycle travel throughout the community.

Policy 8 Improve existing roadways to accommodate new or upgraded bicycle facilities.

Action 8.1 Coordinate with other jurisdictions and outside agencies to study ways to minimize potential bicycle hazards at or near freeway interchanges and roadway intersections.

Action 8.2 Include appropriate bicycle-related improvements as a condition of design review or subdivision approval.

Action 8.3 Provide routine accommodations for all modes of travel when implementing City street-widening projects and encourage routine accommodations for all other street improvement projects, where possible.

Action 8.4 Consider sidewalk widths greater than 10 feet when cyclists are allowed and/or encouraged to use sidewalks as a Class III designated facility.

Action 8.5 Coordinate with other local and regional agencies to develop a signage and wayfinding plan that can be implemented to promote bike use and improve access to destinations.

Action 8.6 Designate "Bike Thru Lanes" and "Share the Road" facilities, where possible, on select Class III facilities and at major roadway intersections to promote awareness and provide a safe location for bicyclists.

Action 8.7 Design new and upgraded bicycle facilities to exceed minimum safety criteria, where possible, in order to encourage expanded bicycle use.

Policy 9 Improve bicycle facilities to achieve safe, efficient connectivity while minimizing impacts to users of other transportation modes.

Action 9.1 Integrate new bike facilities when designing new or modifying existing roadways, where possible.

Action 9.2 Maintain the existing off-road trail network for limited bicycle use in designated open space lands, such as Shell Ridge.

Action 9.3 Identify alternative improvements that could help minimize the loss of on-street parking, if possible, where new Class II bicycle facilities are planned.

Action 9.4 Develop a Complete Streets policy that includes vehicle, pedestrian and bicycle related improvements, for upgraded roadways in Walnut Creek.

Action 9.4

Develop a Complete Streets policy that includes vehicle, pedestrian and bicycle related improvements, for upgraded roadways in Walnut Creek.

Policy 10 Develop a continuous bikeway network that closes gaps in the existing system and serves important destinations.

Action 10.1 Enhance network connectivity between transit stops and major destinations, including the Core Area and the City's open space areas.

Action 10.2 Support the development of a bicycle network that provides connections to facilities in neighboring communities.

Action 10.3 Encourage that bicycle routing be included as an integral part of the street design so that bicycle facilities form an integrated, cohesive network.

Action 10.4 Investigate the feasibility of a multi-user bridge over Ygnacio Valley Road for the Mt. Diablo-Briones regional trail near John Muir Medical Center.

Action 11.1

Revise the bicycle parking standards in the zoning ordinance to include bicycle parking for multi-family residential uses.

Policy 11 Increase Availability of Bicycle Parking.

Action 11.1 Revise the bicycle parking standards in the zoning ordinance to include bicycle parking for multi-family residential uses.

Action 11.2 Adopt standards for the installation and placement of bicycle racks and lockers.

Action 11.3 Expand the number of bicycle racks and lockers in parking garages, employment centers, shopping centers, transit stations and the Core Area to meet future demand.

Action 11.4 Consider the adoption of an in-lieu bicycle parking fee for developers in order to provide flexibility in meeting the bike parking requirement.

4 | Goals and Policies

THE ORIGINAL CBPP ESTABLISHED a set of goals and policies to guide the Authority's actions and decisions in implementing the plan and, more generally, in supporting walking and bicycling in Contra Costa. This chapter refines and clarifies the original goals and policies, in part to stress the Authority's special role as a countywide transportation planning, funding and coordinating agency. It also includes an overarching vision statement (at right) and new objectives for each, neither of which included in the 2003 CBPP.

The goals and policies define the roles and responsibilities of the Authority in implementing the CBPP. They serve as the basis for the more detailed implementation tasks, actions and prioritization criteria outlined in Chapter 9, "Implementation." In many cases, the Authority will need to rely on the cooperation of local agencies to pursue the goals and policies described here. Local jurisdictions, which include the County, cities and towns and special districts, are primarily responsible for implementing pedestrian and bicycling programs and for planning, designing and constructing facilities. As such, these ju-

risdictions will play an essential role in achieving the vision of the CBPP.

VISION STATEMENT

The vision statement is the expression of what walking and bicycling conditions will be like in Contra Costa if the Authority successfully implements the CBPP. The vision statement for the CBPP is:

More people who live, work, shop and go to school in Contra Costa will walk and bicycle, thereby improving health, reducing emissions of greenhouse gases and making our transportation system more sustainable. To support walking and bicycling, Contra Costa will have an integrated system of safe, convenient and comfortable pedestrian and bicycle facilities that provide access to schools, jobs, transit, shopping, neighborhoods, community facilities, parks and regional trails. Agencies within Contra Costa will collaborate on creating such facilities across jurisdictions and will accommodate the needs of pedestrians and bicyclists when planning, designing, building and maintaining all development and transportation projects.

Below are the five goals of the CBPP, each followed by an objective that the Authority will use to measure progress toward achieving that goal and policies that will guide the Authority toward that goal. The goals and policies are meant to apply to the Authority while achieving the objectives will require the concerted effort of all local jurisdictions and other public agencies in Contra Costa.

GOAL 1: EXPAND, IMPROVE AND MAINTAIN FACILITIES FOR WALKING AND BICYCLING

Contra Costa already possesses numerous pedestrian and bicycle facilities, including trails of regional importance, several pedestrian-oriented districts and a growing network of bicycle lanes. However, many significant gaps and major barriers remain, which inhibit mobility and access for pedestrians and cyclists. The following policies are meant to guide the development of an integrated system of facilities for nonmotorized transportation that would provide access for pedestrians and bicyclists to shopping, school, work and recreation activities, both within individual cities and throughout Contra Costa.

Objective | Increase the number of bikeway miles and pedestrian-oriented districts in Contra Costa.²

Policy 1.1 | Describe a countywide system of nonmotorized transportation facilities and key destinations and other attractors of pedestrians and bicyclists.

² Appendix B lists Contra Costa’s pedestrian districts (see topic 2 in the appendix). Pedestrian districts are defined as locations where, by policy, local jurisdictions especially want to encourage walking and improve the safety and comfort of pedestrians. Depending on how broadly the definition is applied, there are approximately 20-30 existing or planned pedestrian districts in Contra Costa.

Policy 1.2 | Identify significant gaps and barriers to walking and bicycling and define funding priorities for removing these obstacles and implementing other needed pedestrian and bicycle projects and programs.

Policy 1.3 | Provide funding for the construction and maintenance of priority bicycle and pedestrian facilities to provide access to activity centers and other key destinations; connect cities within Contra Costa; and connect Contra Costa to neighboring counties.

Policy 1.4 | Include the costs to maintain pedestrian and bicycle facilities when estimating the maintenance needs of streets and roads; encourage local jurisdictions to do the same and also to maintain their pedestrian and bicycle facilities.

Policy 1.5 | Discourage jurisdictions in Contra Costa from removing, degrading or blocking access to pedestrian and bicycle facilities, or converting them to motor vehicle use, without providing an alternative that is at least equally safe and convenient.

Policy 1.6 | Work with neighboring counties to ensure that bicycle and pedestrian facilities are coordinated across county lines.

GOAL 2: IMPROVE SAFETY FOR PEDESTRIANS AND BICYCLISTS

Improving safety should be a central objective of every pedestrian and bicycle plan. Motorists, pedestrians and bicyclists need to understand and obey the rules of the road, as heightened consideration of and respect for other users generally leads to safer conditions. In addition to policies to expand and improve facilities for bicycling and walking, implementation of the following policies will help increase the safety of pedestrians and bicyclists.

Objective | Reduce the rate of pedestrian and bicycle fatalities and injuries per capita.

- Policy 2.1** | Give relative funding priority to projects that address safety deficiencies for pedestrians and bicyclists, especially conflicts with motor vehicles.
- Policy 2.2** | Provide funding for traffic calming, intersection improvements and other projects if they improve safety for both pedestrians and bicyclists.
- Policy 2.3** | Assist local jurisdictions in planning and designing safe streets by providing information, tools and other resources.
- Policy 2.4** | Analyze data on traffic collisions involving pedestrians and bicyclists and share this information with local agencies to assist them in identifying and remedying problem locations.
- Policy 2.5** | Support programs that educate drivers, bicyclists, and pedestrians of their rights and responsibilities, as well as pedestrian and bicycle education and safety programs for adults and youth.
- Policy 2.6** | Support the development of “bike trains,” “walking school buses” and “safe routes to school” programs at schools throughout Contra Costa to encourage more students of various ages to walk or bicycle to school.
- Policy 2.7** | Support enforcement by local police departments of laws that aim to protect pedestrians and bicyclists from collisions with motor vehicles.

GOAL 3: ENCOURAGE MORE PEOPLE TO WALK AND BICYCLE

Beyond providing funding for safe and direct routes, the Authority (and other public agencies) can help more people make walking and bicycling everyday activities by providing information, training and encouragement. Maps can help people find appropriate routes, for example, while training on bicycling safety can give people more con-

fidence to ride. The policies below seek to encourage more people in Contra Costa to walk or bicycle and to do so more often.



Objective | Increase the share of all trips made by walking and bicycling in Contra Costa.

- Policy 3.1** | Work with local and regional agencies to develop and implement encouragement and promotion programs for walking and bicycling aimed at a broad range of audiences and potential users.
- Policy 3.2** | Incorporate bicycle- and walking-related services into broader transportation demand management and commute alternatives programs and support events such as “bike to work” days, “walk to school” days and “National Walk at Lunch Day.”
- Policy 3.3** | Support wayfinding programs for pedestrians and bicyclists, such as free maps, trip-planning services, the regional 511 BikeMapperSM program and signage at transit stations; and

work with local agencies to develop a countywide signage scheme, including directional and destination signs for bike-ways and trails and location maps in pedestrian districts.

Policy 3.4 | Provide funding for secure short- and long-term bicycle parking and encourage local jurisdictions to install bicycle lockers and racks at public facilities and on sidewalks in downtowns and to require it as part of new developments.

Policy 3.5 | Support bicycling-skills classes and other programs that help bicyclists learn how to ride safely.



GOAL 4: SUPPORT LOCAL EFFORTS TO IMPROVE CONDITIONS FOR WALKING AND BICYCLING

Building an integrated system of walking and bicycling facilities in Contra Costa will require the collaboration of the County, cities and towns, and other agencies with land use responsibilities. While the Authority has an essential role in funding facilities and programs, local agencies are mainly responsible for planning, designing, constructing and otherwise implementing them. Local jurisdictions can

best accomplish those tasks by first developing their own pedestrian and bicycle plans to help them formulate and prioritize their actions. Below are ways in which the Authority will support and coordinate local efforts.

Objective | **Help every local jurisdiction in Contra Costa adopt a bicycle and pedestrian plan.**

Policy 4.1 | Provide a forum for local agencies and other stakeholders to identify and implement multi-jurisdictional projects and programs and to develop countywide or subregional approaches for resolving pedestrian- and bicycle-related issues of mutual concern.

Policy 4.2 | Continue to sponsor and support the multi-jurisdictional Countywide Bicycle and Pedestrian Advisory Committee (CBPAC), particularly in their efforts to select recommended projects for funding, review "routine accommodation" checklists (see page 96), and provide guidance during the next update of the CBPP.

Policy 4.3 | Inform local agencies of funding opportunities for pedestrian and bicycle projects and provide them with assistance in developing grant applications.

Policy 4.4 | Give relative funding priority to projects with countywide or multi-jurisdictional benefits.

Policy 4.5 | Maintain an updated online directory of best practices, model policies, standards and guidelines, and other resources for local agencies related to the planning, design and implementation of pedestrian and bicycle facilities and programs and pedestrian- and bicycle-friendly developments.

GOAL 5: CONSIDER AND PLAN FOR THE NEEDS OF PEDESTRIANS AND BICYCLISTS

Transportation improvements to facilitate automobile and truck traffic or mass transit can worsen conditions for pedestrians and bicyclists if their needs are not considered in advance. The same is true for new land use development or redevelopment projects. Retrofitting established land uses and roadways with limited right-of-way to accommodate pedestrians and bicyclists is challenging and costly, and produces unsatisfactory compromises. For this reason, all new transportation and land use projects should incorporate facilities for nonmotorized travel—including temporary ones needed during construction—from the earliest stages of development. At a minimum, new projects should not remove, degrade or cut off access to existing or planned facilities. Where demand for nonmotorized travel is low or the cost of facilities would exceed 20 percent of the cost of the larger project,³ agencies should consider creative and alternative ways to accommodate the needs of pedestrians and bicyclists in the area.

Objective | Help every local jurisdiction in Contra Costa adopt and begin implementing effective policies and standards for pedestrian- and bicycle-friendly developments.

Policy 5.1 | Encourage local jurisdictions to consider the impacts of their development decisions on walking and bicycling and, consistent with the Authority's Growth Management Program, require the jurisdictions to adopt policies and standards that

support pedestrian, bicycle and transit access in new developments.

Policy 5.2 | Monitor capital improvement projects to ensure that the needs of pedestrians and bicyclists (including children, seniors and persons with disabilities) are considered in programming, planning, design, construction, operation and maintenance activities and products; encourage local agencies to do the same.

Policy 5.3 | Comply with the "routine accommodation" requirements of the Metropolitan Transportation Commission concerning the evaluation of needs for pedestrian and bicycle facilities, and assist local implementing agencies in meeting their responsibilities.

Policy 5.4 | Require that roadway projects funded by the Authority incorporate "complete streets" principles as appropriate so that they provide safe and convenient access to bicyclists and pedestrians, among other users.

Policy 5.5 | For transportation projects funded by the Authority that result in the removal or degradation of pedestrian or bicycle facilities, provide at least equally safe and convenient alternatives.

Policy 5.6 | For transportation projects funded by the Authority, provide temporary accommodations for pedestrians and bicyclists during construction activities.

³ The U.S. Department of Transportation's policy statement on routine accommodation (see page 30) considers "the cost of establishing bikeways or walkways [to] be excessively disproportionate to the need or probable use" if it exceeds 20 percent of the cost of the larger transportation project.



Application Form

Measure J Transportation for Livable Communities (CC-TLC) Program Funds

1. PROJECT INFORMATION

Project Name Contra Costa Centre Treat Boulevard/I-680 Bicycle and Pedestrian Plan (Plan)

Project Location Unincorporated Contra Costa County/City of Walnut Creek

Sponsor Contra Costa County

Implementing agency Contra Costa County/City of Walnut Creek

Partner agencies City of Walnut Creek

Contact for project Jamar Stamps, Planner, 925-335-1220,
jamar.stamps@dcd.cccounty.us, 651 Pine Street, 4th Floor North Wing, Martinez, CA 94553

2. FUNDING

In thousands of dollars, please provide the estimated cost of the proposed project, the amount of CC-TLC funds requested, and any other funds that have been committed to the project.

<i>Source</i>	<i>Amount</i>
Total project cost †	\$100,000
Requested CC-TLC funds	\$100,000
Other committed funding 1 ‡	\$000
Other committed funding 2 ‡	\$000
Unfunded balance	\$100,000

† Minimum request is \$50,000
 ‡ Add other committed funding sources as needed

3. POTENTIAL PHASING

{Identify project components that could be eliminated if insufficient funding is available to fully fund the project as proposed and the estimated reduction in project cost and requested Measure J funds}

The Plan development could generally be phased as follows:

- **Project Startup:** The County and Walnut Creek staff would meet with CCTA to discuss the timeline and request for proposals for the project.
- **Public/Stakeholder Engagement:** Conduct outreach to affected stakeholders and members of the public using the Contra Costa Centre Municipal Advisory Council and the Walnut Creek Bicycle Committee as the forum for public engagement. Possibly conduct workshops to prepare existing conditions report, identify alternatives and determine recommended improvements.
- **Develop Alternatives:** Develop Plan concepts and proposed improvement projects for the Treat Boulevard/I-680 over-crossing.
- **Draft Plan Document:** Use information gained from public outreach and Plan development and analysis to create a draft Plan.
- **Refinement/Final Plan:** Analyze comments and discussions from draft Plan process to develop the final Plan document.

Staff believes that these phases would be necessary to deliver the project. However County staff and CCTA staff would meet to discuss any modifications to the project phasing that may be necessary.

4. PROJECT DESCRIPTION

{In a single paragraph, provide a short description of the project and its purpose, location and key components}

The proposed project would provide a plan that identifies improvements to serve bicyclists and pedestrians using the Treat Boulevard/I-680 over-crossing near the Contra Costa Centre/Pleasant Hill BART station area. The Treat Boulevard/I-680 over-crossing is one of the main arteries into the Contra Costa Centre/Pleasant Hill BART station area from areas of Walnut Creek west of the freeway. The proposed project area encompasses the portion of Treat Boulevard from North Main Street (City of Walnut Creek) east to the Iron Horse Trail (Contra Costa County). The County/City limit boundary is staggered across the freeway over-crossing.

Key components of the proposed project include, but are not limited to:

- Analysis of Existing Conditions;
- Preliminary Transportation and Urban Framework Improvements;
- Urban Design Concepts;
- Evaluation of Feasibility of Proposed Improvements;
- Draft and Final Plan.

Participation from key stakeholders (participating jurisdictions/agencies, community, etc.) will occur throughout the development of the Plan.

{Describe the proposed project more fully including the project purpose, its features, its location and the how the sponsoring or implementing agency intends to operate and maintain the project. At a minimum, attach a location map. Photos and designs may be included as well.}

The project would provide a plan that identifies improvements to serve bicyclists and pedestrians using the I-680/Treat Boulevard over-crossing near the Contra Costa Centre/Pleasant Hill BART station area. The Treat Boulevard/I-680 over-crossing is one of the main arteries into the Contra Costa Centre/Pleasant Hill BART Specific Plan area from areas of Walnut Creek west of the freeway. The Contra Costa Centre/Pleasant Hill BART Area Specific Plan identifies a need for a future bicycle and pedestrian circulation route along this segment (Figure 7.3 shown below). The general study area is encompassed within the red circle on Figure 7.3, and the Proposed Study Area Map following Figure 7.3. The study area will also include the section of Treat Boulevard between I-680 and North Main Street to the west in the City of Walnut Creek.

The Contra Costa Centre/Pleasant Hill BART Specific Plan contains policies and recommendations that encourage improved bicycle and pedestrian circulation access to, through, and from the Specific Plan area. In turn, the proposed Plan would support and help implement these policies and recommendations. In addition, the City of Walnut Creek adopted policies in their General Plan 2025 that support this project. The following policies are also attached to this application.

Contra Costa County General Plan 2005-2020

- Policies for the Pleasant Hill BART Station Area, 3-108(f): Develop areas intensively used by pedestrians at a human scale and with adjoining uses which will visually and functionally enliven the area.

Pleasant Hill BART Station Area Specific Plan

- Transportation and Circulation: Plan Objective 3 – Improve automobile access to the BART station from Pleasant Hill, Walnut Creek, and Concord.
- Pedestrian, Bicycle, and Transit Circulation: Bicycle and pedestrian circulation shall be provided through the Station Area. Bicycle and pedestrian circulation shall be safe and shall, to the extent possible, minimize conflicts with other transportation modes.

The Pleasant Hill BART Specific Plan Study (Traffic Study)

- Alternative Modes of Access – Pedestrian Promenade: A promenade for use by pedestrians and bicyclists is recommended to provide access through the area. This promenade would discourage automobile use for employees when they leave their workplaces for lunch, personal business and work-related errands.
- Direct Linkage to Regional Bike Trails: Bicycle paths could be connected to the extensive regional network of bicycle trails to encourage bicycle access to the Specific Plan area. Of immediate use would be a connection to the Iron Horse Trail. With participation of other local agencies, secondary connections could also be made to the California Hiking and Riding Trail in Concord and the Chilpancingo Parkway in Pleasant Hill.

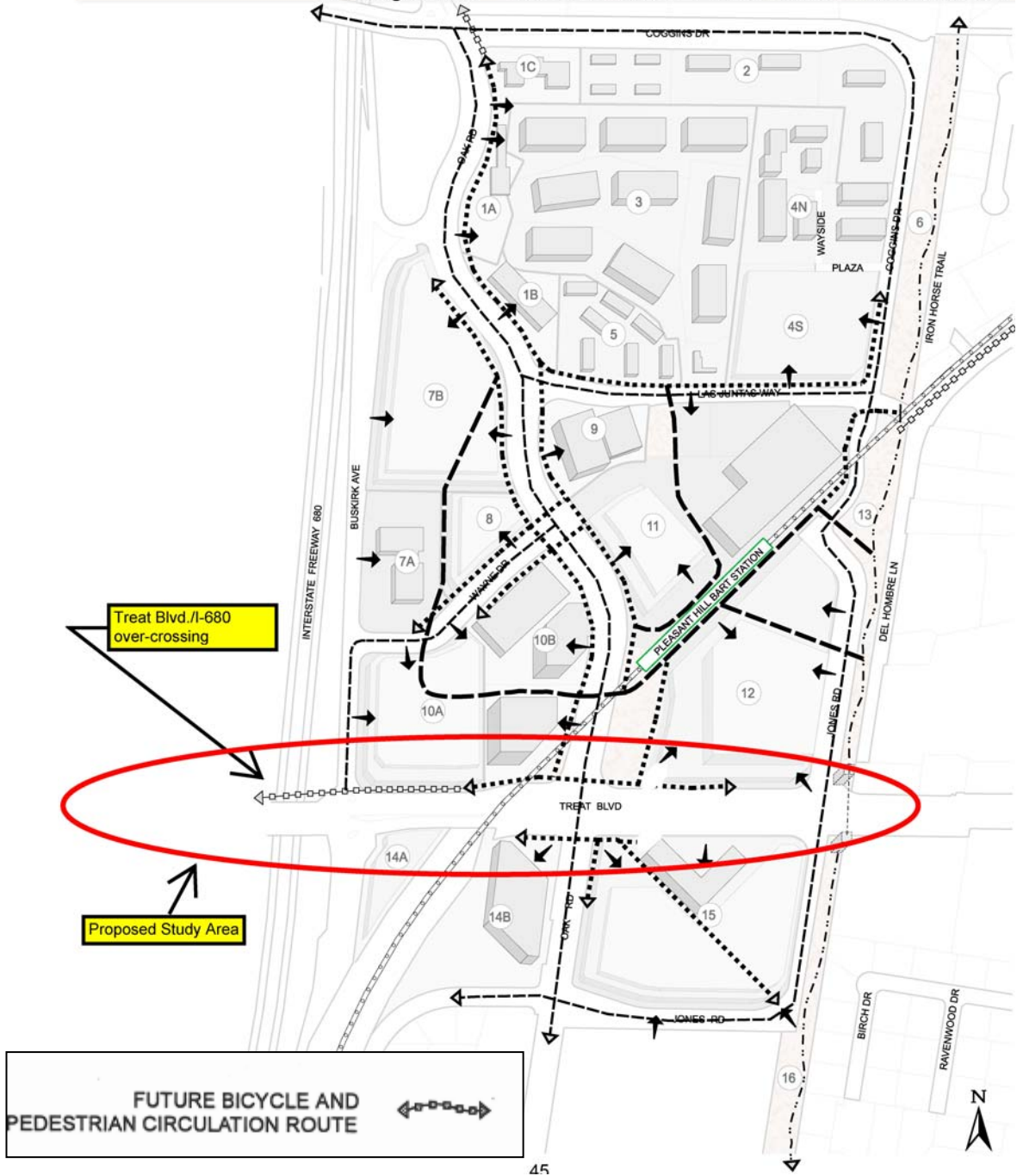
Walnut Creek General Plan 2025

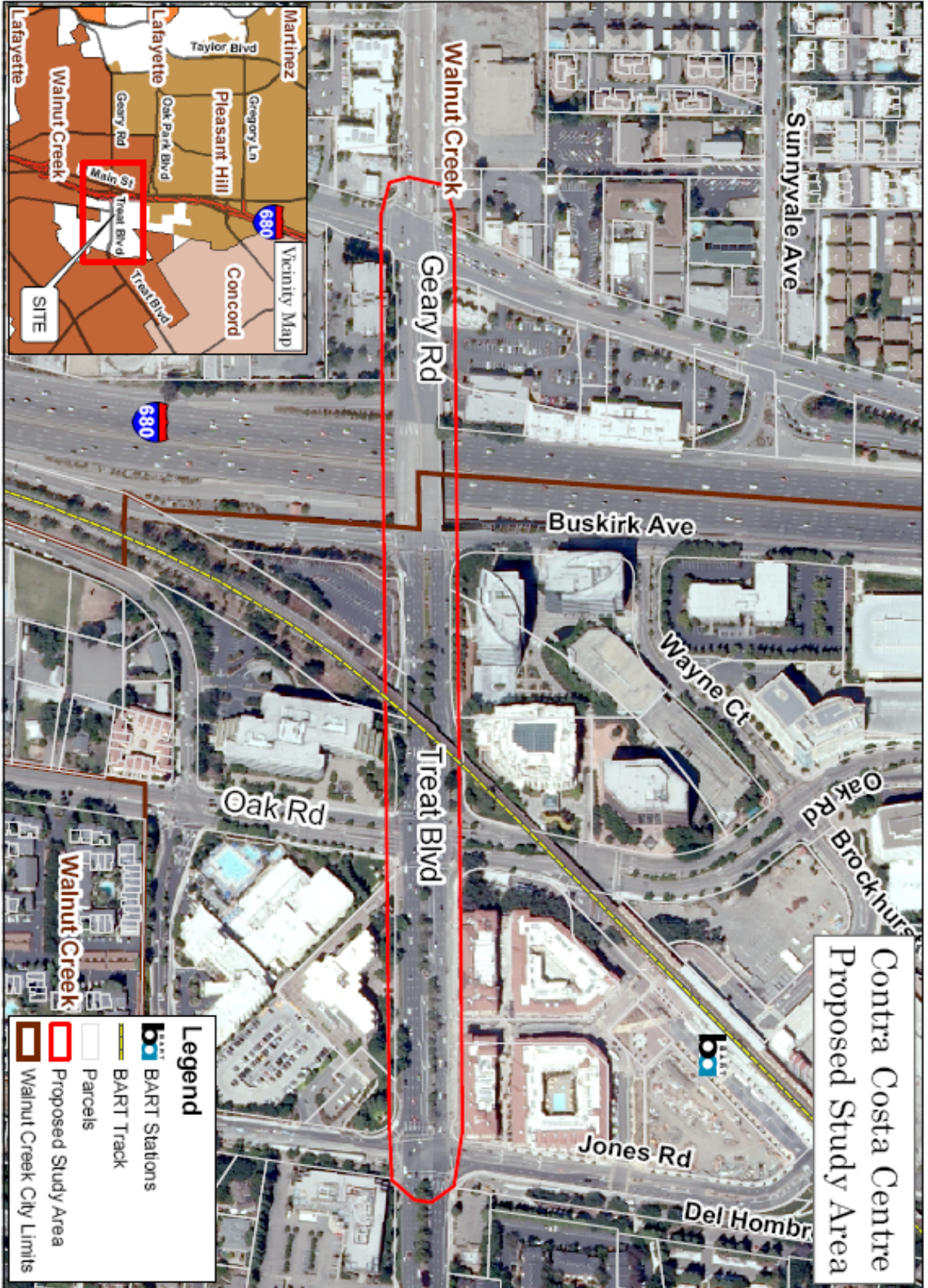
- Policy 23.3: encourage development around the Pleasant Hill BART station that supports the County’s specific plan goals for well-designed, transit-, pedestrian-, and bicycle-oriented development.
- Action 23.3.1: Work with the County toward ensuring that development of the Pleasant Hill BART station area is compatible with and accessible to adjacent areas within the incorporated city.

Excerpt from Contra Costa Centre/Pleasant Hill BART Station Area Specific Plan

URBAN DESIGN POLICY DIAGRAM

Figure 7.3 BICYCLE AND PEDESTRIAN CIRCULATION





Map created 11/02/2011
 by Contra Costa County Department of Information and Development
 Contra Costa County Department of Information and Development
 601 Pine Street, Suite 300, Walnut Creek, CA 94596-0000
 925.938.3100, 925.938.3101, 925.938.3102, 925.938.3103, 925.938.3104
 The map contains copyrighted information and may not be altered. It may be
 reproduced in its current state if the source is cited. Users of the map agree to read and
 accept the County of Contra Costa disclaimer of liability for geographic information.



North Side of Treat Boulevard/I-68o Over-Crossing Looking East Toward Contra Costa Centre



South Side of Treat Boulevard/I-68o Over-Crossing Looking South East Toward Contra Costa Centre



Corner of North Main Street/Treat Boulevard Looking East Toward Contra Costa Centre
(Notice Sidewalk Approaching Southside of Over-Crossing Does Not Exist)



Southbound I-680 On-Ramp from Treat Boulevard Looking East Toward Contra Costa Centre



Northbound I-680 Off-Ramp (Truck on Left)/Treat Boulevard Looking West Toward Walnut Creek



South Side of Treat Boulevard Looking East, Across from Contra Costa Centre (Notice Sidewalk Abruptly Ends)



5. PROPOSED SCHEDULE

In the following table, enter the expected beginning and ending *month and year* for each of the phases listed. If the phase has been finished, enter "Completed" in the "To" column.

<i>Phase</i>	<i>From</i>	<i>To</i>
Preliminary Design & Planning	n/a	n/a
Design	n/a	n/a
Environmental Clearance	n/a	n/a
Right-of-Way	n/a	n/a
Construction	n/a	n/a

This type of scheduled phasing pertains to constructions projects. The proposed planning study can not be phased in such a manner. However, if the project is awarded, scheduled milestone dates for project development could be generally shown as follows:

- April 2012: Receive CC-TLC allocation
- April 2012: Project Initiation
- April- September 2012: Prepare Feasibility Study and Recommendations
- October-December 2013/14: Draft Plan and Public Review
- January-March 2014: Final Plan

6. ABILITY TO MEET CRITERIA

Describe how the proposed project would meet the following four criteria:

Achievement of CC-TLC

Goals: Identify which of the six goals of the CC-TLC program the proposed project would help achieve – a project need not meet all of them – and *briefly* describe how it would do so (see Exhibit One for these six goals)

Help create walkable, pedestrian-friendly neighborhoods and business districts

With the Plan implemented, residents and workers in the Contra Costa Centre/Pleasant Hill BART Station area would be able to take advantage of the amenities (restaurants, shopping, etc.) across the I-680 freeway in Walnut Creek by utilizing an improved over-crossing. Currently, there is no sidewalk on the south side of the over-crossing and there are no designated bicycle facilities connecting the Geary Road bike lanes west of the over-crossing to the Iron Horse Trail east of the over-crossing. The Plan will develop recommendations for bicycle and pedestrian improvements that would create a safe and efficient access from the BART station area, to adjacent areas west of the BART station. The Plan will develop recommendations that can create an overall safer transportation environment for bicyclists and pedestrians across the northbound I-680 off ramp and Buskirk Avenue.

Promote innovative solutions, including compact building design and context-sensitive site planning that is integrated with the transportation system

The Contra Costa Centre/Pleasant Hill BART Station area is a transit-oriented development (TOD), which locates employment and housing next to a regional transit hub. The project is largely complete and improvements for bicyclists and pedestrians on the freeway over-crossing are one of the last remaining measures to implement.

Help create walkable, pedestrian-friendly access linking housing and job centers to transit

The intent of the Plan would provide recommendations that would create safer facilities for bicycle and pedestrian traffic to travel back and forth from the Contra Costa Centre/Pleasant Hill BART Station area across the Treat Boulevard/I-680 over-crossing to neighborhoods and amenities west of the freeway. The bicycle and pedestrian environment in this area is limited, and favors automobile movement.

Help create affordable housing

The Plan would improve the bicycle and pedestrian environment across the freeway separating the Contra Costa Centre/Pleasant Hill BART station and adjacent areas in Walnut Creek. Improving this main point of access would enhance the walkability of the two areas, and therefore further encourage TOD development west of the freeway and potentially increase the ratio of available affordable housing.

Encourage a mixture of land uses and support a community's development or redevelopment activities

The Contra Costa Centre/Pleasant Hill BART Station Area Specific Plan has policies that would be supported by the proposed Plan (see: "Project Description").

Provide for a variety of transportation choices to enhance a community's mobility, identity, and quality of life

The Contra Costa Centre/Pleasant Hill BART TOD is a major transportation hub for Contra Costa County. There are a variety of transportation options available, rail, bus, and the Iron Horse Regional Trail. The plan would identify improvements that would make the freeway less of

	<p>a barrier for pedestrian and bicyclists who need to travel across the freeway to reach these transportation services. The plan will also evaluate improvements that help buffer pedestrians and bicyclists from automobile traffic on the over-crossing.</p>
<p>Feasibility: Describe why the project is feasible and any outstanding issues in the project development process – design, environmental clearance, right-of-way purchase, and PS&E – or funding of the project</p>	<p>Currently the project is in the planning stages. There have been no commitments to certain types of improvements. The Plan addresses Contra Costa Centre/Pleasant Hill BART Specific Plan policy and addresses a need to improve the bicycle and pedestrian access across the Treat Boulevard/I-680 over-crossing.</p>
<p>Local and policy support: Identify local policies that support the project and the integration of the project with other local efforts, as well as other support from the general public, the RTPCs and other relevant agencies</p>	<p>Section 2.a. under “Project Description” above lists the supporting policies. Letters of support (attached) have been provided by the Contra Costa Centre Municipal Advisory Council, Contra Costa Centre Association, Walnut Creek Bicycle Advisory Committee, and the Walden Improvement Association. On a staff level, the Plan currently is also supported by the City of Walnut Creek and Transpac TAC. Once the County receives funding to develop this planning document, County staff will seek approval from the respective elected bodies of the affected agencies.</p>
<p>Matching funds: Identify any funds from other sources that are or would be committed to the project</p>	<p><u>{What percentage of non-Measure J funds would be committed to the project?}</u></p> <p>The County has not proposed a source of matching funds for the proposed planning study.</p>

att: Letters of Support

EXHIBIT ONE

Goals of the Measure J CC-TLC Program

Measure J establishes six goals for the Measure J Transportation for Livable Communities program.

- Help create walkable, pedestrian-friendly neighborhoods and business districts
- Promote innovative solutions, including compact building design and context-sensitive site planning that is integrated with the transportation system
- Help create walkable, pedestrian-friendly access linking housing and job centers to transit
- Help create affordable housing
- Encourage a mixture of land uses and support a community's development or redevelopment activities
- Provide for a variety of transportation choices to enhance a community's mobility, identity, and quality of life

Walden District Improvement Association

October 18, 2011

Mr. Jamar I. Stamps, Planner
Department of Conservation & Development
Transportation Planning Section
Contra Costa County
651 Pine St., 4th Floor - North Wing
Martinez, CA. 94553-1229
Via email: Jamar.Stamps@dcd.cccounty.us

RE: CCTA-TLC Grant Application Treat Boulevard/I-680 Overcrossing

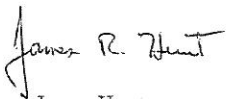
Dear Mr. Stamps:

Walden District Improvement Association represents the residents in the unincorporated area of Contra Costa County surrounding the Pleasant Hill/Contra Costa Centre BART Station. We are an active participant in the evolution of a BART station isolated by expansive surface parking lots into a mixed use transit village. I am writing to offer our support for your application to the Contra Costa Transportation Authority program on Transportation for the Livable Communities (CCTA-TLC) that will follow through on a Pleasant Hill BART Specific Plan policy.

Walden recognizes the improvements in pedestrian and bicycle circulation to the east of the I-680 freeway, but the continued difficulty in crossing the freeway along Treat Blvd. We strongly support the application for a planning grant and look forward to working with the Department of Conservation and Development in evaluating options that tie our walking and bicycling communities more closely together.

Please let me know how Walden can assist in the proposal process and then in building community consensus on paths forward.

Sincerely,



James R. Hunt, President
2632 Cherry Lane
Walnut Creek, CA 94597
Email: hunt@ce.berkeley.edu; phone: 925 451 2761

cc: Laura Case with County Supervisor Mitchoff's office
Brian Amador, Chair, Contra Costa Centre Area Municipal Advisory Council

MAC
Contra Costa Centre Area
Municipal Advisory Council
C O N T R A C O S T A C O U N T Y

Mr. Jamar I. Stamps
Planner Department of Conservation & Development
Transportation Planning Section
Contra Costa County
651 Pine St., 4th Floor - North Wing
Martinez, CA. 94553-1229
Via email: Jamar.Stamps@dcd.cccounty.us

October 25, 2011

RE: CCTA-TLC Grant Application Treat Boulevard/I-680 Overcrossing

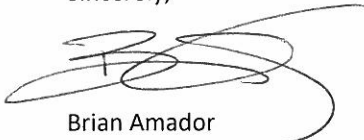
Thank you for presenting the CCTA- TLC Grant Application Treat Boulevard/I-680 Overcrossing project to the Contra Costa Centre Area Municipal Advisory Council (CCC MAC) during our meeting on October 17. Following the presentation, the CCC MAC members first moved, then voted to recommend support for the study phase of the project.

We recognize the need for developing a safe and effective passage for pedestrians and cyclists to cross the 680 freeway at Treat Boulevard.

Should the project move forward, the CCC MAC has requested the opportunity to review plans as they develop. Should the need arise; please contact me for placement on the CCC MAC agenda.

Thank you once again, Mr. Stamps

Sincerely,



Brian Amador
Contra Costa Centre MAC Chair

c/o Renaissance ClubSport Walnut Creek
2805 Jones Road
Walnut Creek, CA 94597

Ph: 925 942-6302
e-mail: bamador@leisuresportsinc.com

cc:
CCC MAC Board

Laura Case,
Deputy Chief of Staff
Office of Supervisor Karen Mitchoff

Contra Costa Centre Association

1350 Treat Boulevard, Suite 180
Walnut Creek, CA 94597
Phone (925) 935-6337
Fax (925) 935-1407
ccca@contracostacentre.com
www.contracostacentre.com



November 7, 2011

Mr. Jamar I. Stamps, Planner
Department of Conservation and Development
Transportation Planning Section
Contra Costa County
651 Pine Street, 4th Floor- North Wing
Martinez, CA. 94553-1229

RE: Letter of Support for I-680/Treat Boulevard Bicycle and Pedestrian Improvement Plan

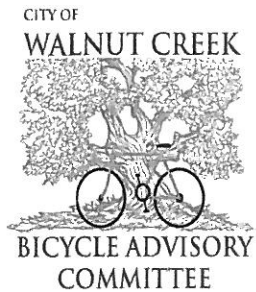
Dear Mr. Stamps,

The Contra Costa Centre Association is very supportive of the Contra Costa County's application for a planning grant to the Contra Costa Transportation Authority, Transportation for Livable Communities program for funding to be used to develop a bicycle and pedestrian improvement plan. This improvement plan for the section of Treat Boulevard between the Iron Horse Trail and North Main Street would help develop and improve access for pedestrian bicycle access for the residents and the employees. The Contra Costa Centre Transit Village promotes the use of alternative transportation through a Transportation Demand Management Program, of which 32% of the 6,000 employed at the Contra Costa Centre Transit Village take an alternative to the worksite.

The Centre Association looks forward to working with the County on this project and will provide any material concerning the Contra Costa Centre TDM program that would help implement the proposed Plan by the Contra Costa County.

Sincerely,

Lynette Busby
Executive Director
Contra Costa Centre Association



November 10, 2011

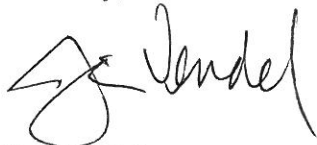
Mr. Brad Beck
Senior Transportation Planner
Contra Costa Transportation Authority
2999 Oak Road, Suite 100
Walnut Creek CA 94597

Dear Mr. Beck,

The Walnut Creek Bicycle Advisory Committee (BAC) fully supports Contra Costa County's Transportation for Livable Communities (TLC) grant application for the Treat Boulevard and Interstate 680 planning grant to study options that improve the bicycle/pedestrian connectivity between the Pleasant Hill BART Station and North Main Street in Walnut Creek.

Treat Boulevard is a roadway identified in the City of Walnut Creek's Bicycle Master Plan, the Countywide Bicycle and Pedestrian Master Plan, and the Contra Costa Centre Specific Plan for improved access to the Pleasant Hill BART Station. Currently pedestrians and bicyclists alike have difficulty using the I-680 over-crossing, as the existing bridge width and lack of sidewalks limit opportunities for safe access between the residential and commercial areas in Walnut Creek and the BART Station. The Walnut Creek Bicycle Advisory Committee believes this planning study is necessary to explore the various engineering and funding options that need to be considered in order to provide both pedestrians and bicyclists with safe mobility and access across the I-680 freeway.

Sincerely,



Jim Vendel
Chairperson
Walnut Creek Bicycle Advisory Committee



Application Form

Measure J Transportation for Livable Communities (CC-TLC) Program Funds

1. PROJECT INFORMATION

Project Name Northwest Walnut Creek Pedestrian and Bicycle Access

Project Location Buena Vista Avenue (Geary Rd-Parkside Dr)
 Parkside Drive (Buena Vista Ave-Hillside Dr)
 Hillside Drive (Parkside Dr-East end of 680 overpass connecting
 to existing sidewalk to BART)
 San Juan Avenue (Alvarado Ave to south end of Elementary
 school property line)

Sponsor..... City of Walnut Creek

Implementing {if different than sponsor}
 agency.....

Partner agencies Caltrans
 Walnut Creek School District

Contact for project... Yun Na Rhee, Assistant Engineer, (925) 256-3529, rhee@walnut-creek.org, 1666 N. Main St, Walnut Creek, CA 94596

2. FUNDING

In thousands of dollars, please provide the estimated cost of the proposed project, the amount of CC-TLC funds requested, and any other funds that have been committed to the project.

<i>Source</i>	<i>Amount</i>
Total project cost †	\$1,250,000
Requested CC-TLC funds	\$1,000,000
City funding-local match †	\$250,000
Other committed funding 2 †	\$000
Unfunded balance	\$0

† Minimum request is \$50,000

‡ Add other committed funding sources as needed

3. POTENTIAL PHASING

{Identify project components that could be eliminated if insufficient funding is available to fully fund the project as proposed and the estimated reduction in project cost and requested Measure J funds}

The project has multiple components of improvements such as bicycle improvements, sidewalk improvements, corner bulb out improvements, restriping, signage etc. These improvements are segmented which can provide the opportunity for it to be eliminated if needed depending on the level of limited funds that are available. We would however attempt to maximize use of funding for improvements to this area of the City to capitalize on multi-level modes of transport for this northwest area of the City.

4. PROJECT DESCRIPTION

{In a single paragraph, provide a short description of the project and its purpose, location and key components}

The project is approximately 2 miles of improvements including but not limited to bicycle sharrows, signage, sidewalk improvements, installation of curb ramps, traffic calming through means of correcting non conforming perpendicular spaces in the frontage of a school, pedestrian signal and sidewalk installations. All of these improvements connect to multiple locations—Walnut Creek BART, Pleasant Hill BART, churches, parks, museum, downtown shopping, schools, neighborhoods, central Walnut Creek etc.

{Describe the proposed project more fully including the project purpose, its features, its location and the how the sponsoring or implementing agency intends to operate and maintain the project. At a minimum, attach a location map. Photos and designs may be included as well.}

The project is proposing to fill the gap for a bicycle/pedestrian connection to an existing pedestrian route through 1.5 miles of residential neighborhoods that presently includes pedestrian walkways through the neighborhood leading towards the I-680 freeway underpass which currently has no pedestrian connection, no protected crossing at the I-680 SB on ramp and no connection to an existing sidewalk to the Walnut Creek BART station. The grant funds would provide the means to connect 1.5 mile path for bicyclists and pedestrians with the installation of bicycle sharrows from the residential neighborhoods connecting the pedestrian/bicycle gap from the western side of the 680 underpass with a sidewalk connection to an existing sidewalk on the eastern side of the I-680 underpass with sharrows and a protected pedestrian signal which would complete a connection to the WC BART station approximately a quarter of a mile away.

To the east of these proposed improvements is San Juan Avenue between Alvarado Avenue to South property border of Buena Vista Elementary School. This school located to

the northwest of the downtown district on the block bordered by San Juan Avenue, Buena Vista Avenue, Alvarado Avenue and Parkside Drive. The project site would be to close the pedestrian gap on San Juan Avenue and Alvarado Avenue construct new sidewalk, modify on-street perpendicular parking stalls to 45 degree stalls, provide curb ramps and sidewalk to intersection connecting to the new sidewalk, an associated crosswalk and adjust centerline to realign with the existing centerline of the street. (please see Exhibit 1 and 3 for map and photos of project areas)

5. PROPOSED SCHEDULE

In the following table, enter the expected beginning and ending *month and year* for each of the phases listed. If the phase has been finished, enter "Completed" in the "To" column.

<i>Phase</i>	<i>From</i>	<i>To</i>
Preliminary Design & Planning	April 2012	August 2012
Design	August 2012	October 2012
Environmental Clearance	n/a	n/a
Right-of-Way	October 2012	February 2013
Construction	February 2013	February 2014

6. ABILITY TO MEET CRITERIA

Describe how the proposed project would meet the following four criteria:

Achievement of CC-TLC Goals:

Identify which of the six goals of the CC-TLC program the proposed project would help achieve — a project need not meet all of them — and *briefly* describe how it would do so (see Exhibit One for these six goals)

Help create walk able, pedestrian friendly neighborhoods and business districts along with linking to housing and job centers to transit:

The installation of bicycle sharrows, sidewalk connections, corner improvement, pedestrian signal etc would encourage bicycle and pedestrian use in the neighborhood. The neighborhood and adjacent areas include existing nearby facilities that provide bus services, access to BART, parks, tennis courts, swimming pool, museums, schools, churches etc all of which are available to be accessed by other means than a vehicle.

Promote innovative solutions, including compact building design and context sensitive planning that is integrated with the transportation system:

The project area is mostly a residential neighborhood that has potential to encourage pedestrian and bicycling throughout the neighborhood with its existing amenities. One of the innovative design solution to the limited right of way that exists on the project streets is to provide sharrows which would provide bicycle designation. Because there is limited right of way a sharrow would compromise a provide for a bicycle access.

Help create affordable housing:

Though the project is not directly creating affordable housing, the project proposal to create availabilities to access different facilities/transportation such as BART, CCCTA, schools, parks, churches etc with bicycle sharrows and pedestrian pathway/signal contributes to affordable housing opportunities as the tenants

would have alternative lower cost options when traveling to different facilities in the nearby vicinity.

Encourage a mixture of land uses and support a community’s development or redevelopment activities.

The project will be providing connections to multiple land uses through alternative means of traffic. This can contribute to increase use of existing facilities increasing vitality in the neighborhoods which can attract additional mixture of land uses to the area.

Provide for a variety of transportation choices to enhance a community’s mobility, identity, and quality of life.

If this project is funded, the neighborhood will have bicycle connection through sharrows, improved pedestrian paths leading towards the elementary school and towards the Walnut Creek BART station with a protected pedestrian signal and bus routes that can access two BART stations, parks, churches, schools and downtown Walnut Creek.

Feasibility: Describe why the project is feasible and any outstanding issues in the project development process — design, environmental clearance, right-of-way purchase, and PS&E — or funding of the project

All improvements proposed for the project is feasible. The project consists of providing bicycle sharrows already evaluated to be placed where it meets the design criteria. All proposed sidewalk improvements are already within City right of way. The pedestrian signal located under the 680 overpass is within Caltrans right of way and the City has already communicated with Caltrans and received a letter agreeing to work with the City if funding is provided. Frontage improvements in front of the school is already within City right of way, however we are already in communications with the school and received support for the proposed improvements.

Local and policy support: Identify local policies that support the project and the integration of the project with other local efforts, as well as other support from the general public, the RTPCs and other relevant agencies	Walnut Creek Bicycle Master Plan 2011 (please see Exhibit 2) Contra Costa Countywide Bicycle and Pedestrian Plan (2009) (please see Exhibit 2) Walnut Creek General Plan 2025 (please see Exhibit 2)
---	--

Matching funds: Identify any funds from other sources that are or would be committed to the project	{What percentage of non-Measure J funds would be committed to the project?} 20%
--	--

PROPOSED IMPROVEMENTS EXHIBIT 1



LEGEND:

- PROPOSED BIKE PATH (SHARROWS OR SHARE THE ROAD SIGNAGE)
- PEDESTRIAN IMPROVEMENTS
- EXISTING CLASS II BIKE LANE
- EXISTING CCTA BUS STOP

EXHIBIT 2

WALNUT CREEK GENERAL PLAN

neighborhoods, near BART, in neighborhoods near upper North Main Street, and near the local high schools.

The City established the preferential residential permit parking programs to limit on-street parking in specific neighborhoods to vehicles with authorized City permits during designated times. The program has been in place since 1989 and continues to expand.

GOAL 4

Protect residential neighborhoods from through-traffic, speeding, and nonresidential parking.

Policy 4.1. Manage arterial and collector traffic to minimize adverse affects on neighborhoods.

Policy 4.2. Discourage through-traffic on local streets and collectors.

Action 4.2.1. Selectively use alternative street designs to discourage through-traffic.

Policy 4.3. Prevent encroachment of non-residential parking in existing neighborhoods.

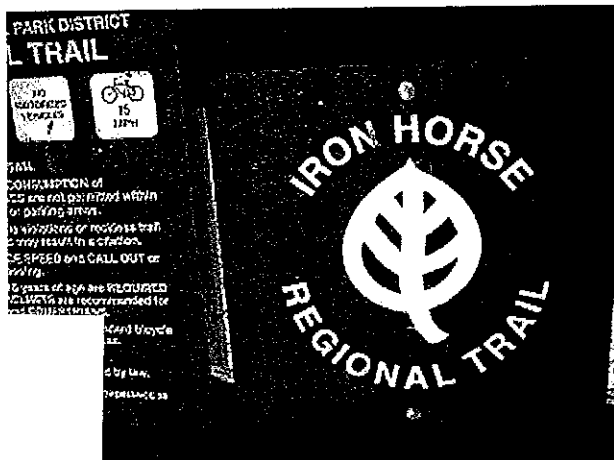
Action 4.3.1. Implement the preferential residential parking-permit program.

Action 4.3.2. Work with schools, particularly the high schools, to develop and implement traffic-calming and shared-parking solutions.

BICYCLING

Bicycling is an important travel mode for social, recreational, and commuter purposes. With its mild climate, relatively level terrain, and proximity of residential, commercial, and business areas, Walnut Creek is particularly suited for bicycling and has an extensive bikeways network. Figure 4 on page 5-9 shows the 2005 and proposed bicycle facilities in Walnut Creek and their connections to other bicycle facilities in the surrounding area.

- **Class 1 bikeways (Bike Path or Bike Trail):** Rights-of-way separated from roadways with minimal automobile cross-flow and a minimum paved width of 8 feet for a two-way path. Class 1 bikeways in Walnut Creek include the Contra Costa Canal, Iron Horse, and Ygnacio Canal trails.
- **Class 2 bikeways (Bike Lane):** Restricted rights-of-way for exclusive use of bicycles, normally striped within the paved areas of roadways and providing a minimum width of 5 feet¹ for one direction. Class 2 bikeways in Walnut Creek include Bancroft Road, Walnut Avenue, and Oak Grove Road.
- **Class 3 bikeways (Bike Routes):** On-street routes designated by signs or other markings. Bikes and cars share the road. Existing Class 3 bikeways include major arterials such as Ygnacio Valley Road from Oakland Boulevard to Walnut Avenue and residential streets like Wiget Lane, Cedro Lane, and Naranja Drive.



Trail signage

GOAL 5

Provide a safe and attractive environment for bicycle travel throughout the community.

- Policy 5.1. Promote bicycle use as an alternative way to get to work, school, shopping, recreational facilities, and transit stops.**
- Action 5.1.1. Develop a bicycle master plan with measurable objectives and incorporate this plan into a new trails master plan. (See Natural Environment Goal 4.)**
- Action 5.1.2. Where feasible, integrate new bikeways and walkways when designing new or modifying existing roadways.**
- Action 5.1.3. When preparing the CIP, include bicycle and bicycle-related projects according to the priorities proposed in the bicycle master plan.**

- Action 5.1.4. Periodically update and distribute a map identifying bike-ways in the city and environs.**
- Action 5.1.5. Pursue grants for constructing and developing new and improved bicycle facilities.**
- Action 5.1.6. As part of the annual report to the City Council on implementation of the general plan, list additions and improvements made to the City's bicycle facilities.**
- Policy 5.2. Provide facilities that encourage and support bicycle travel.**
- Action 5.2.1. Require appropriate bicycle-related improvements as a condition of site development, design review, subdivision, or building permit approval and for all City street-widening projects.**
- Action 5.2.2. Consider sidewalk widths greater than 10 feet whenever bicyclists are allowed or encouraged to use sidewalks.**
- Action 5.2.3. Improve signalized intersections for bicyclist use along highly traveled bicycle corridors.**
- Action 5.2.4. Provide bicycle racks and other bike storage facilities at key high-use public locations.**
- Action 5.2.5. Working with local school districts, plan safe, pleasant, and attractive bicycle routes to school and organize programs that promote bicycling.**

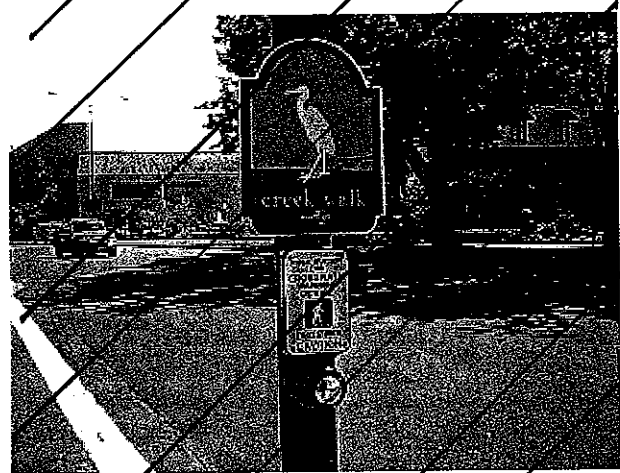
Action 5.2.6. Work with other agencies and jurisdictions to ensure that safe bicycle facilities are available at the edge of the city limits.

Action 5.2.7. Revise design guidelines to require, where appropriate, new projects to provide weather-protected, safe bike parking and/or storage facilities and other bicycle-friendly amenities.

Action 5.2.8. Revise design guidelines to encourage the installation of shower facilities in large, new office developments.

Policy 5.3. Oppose the use of motorized transportation (trains, buses, autos, motorcycles) on the Iron Horse Corridor between the Pleasant Hill BART Station and Newell Avenue.

- **Hiking and walking paths**, such as the Contra Costa Canal, Ygnacio Canal, and Iron Horse trails



Creek walk sign at Broadway

The difference between walkways and sidewalks is primarily in their physical design. To preserve the rural character of specified neighborhoods, the City developed the "walkway" concept: walking areas are separated from vehicular lanes by an asphalt berm or a roadside swale rather than the standard curb and gutter that separates paved streets from concrete sidewalks.

As the City improves the walking environment, it is committed to improving mobility services for the elderly and those with disabilities (e.g., lengthening crossing signal times on crosswalks located near senior housing facilities).

To improve safety, the City built a pedestrian bridge where the Iron Horse Trail crosses Ygnacio Valley Road, vertically separating pedestrians and bicyclists from vehicles. As of 2005, the County was designing a bridge to take the same trail over Treat Boulevard near the Pleasant Hill BART station.

WALKING

Pedestrian walkways range from downtown sidewalks to unpaved shoulders along residential streets to paved trails separated from the roadway. Walkways are particularly important downtown, in commercial areas, and around schools, transit stops, and transit stations.

The City generally requires that sidewalks be installed at the time of development. However, the burden to install as well as maintain sidewalks in most cases rests with the property owner.

Walnut Creek has three types of pedestrianways.

- Sidewalks in urban and suburban areas
- Walkways in rural and semi-rural areas

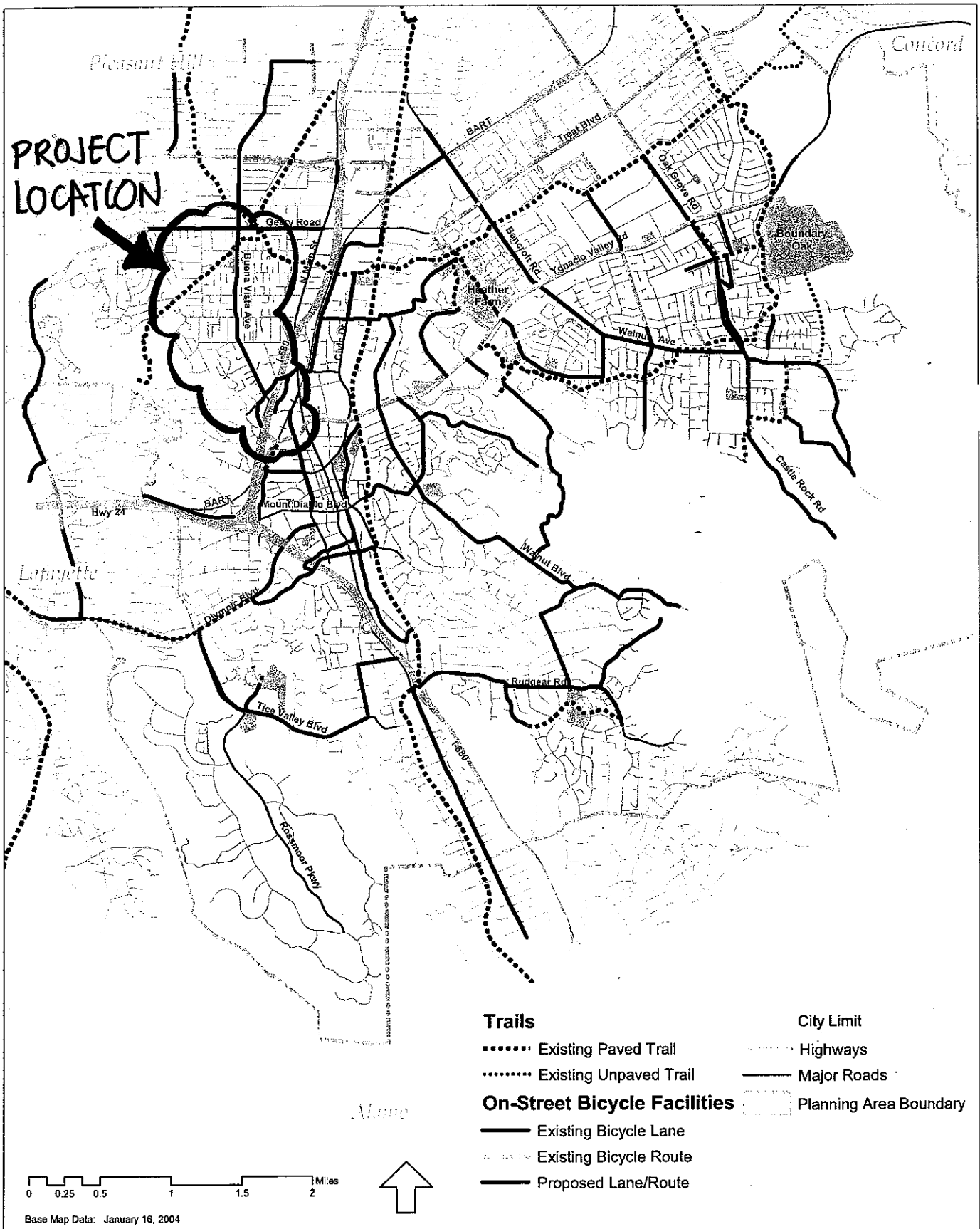
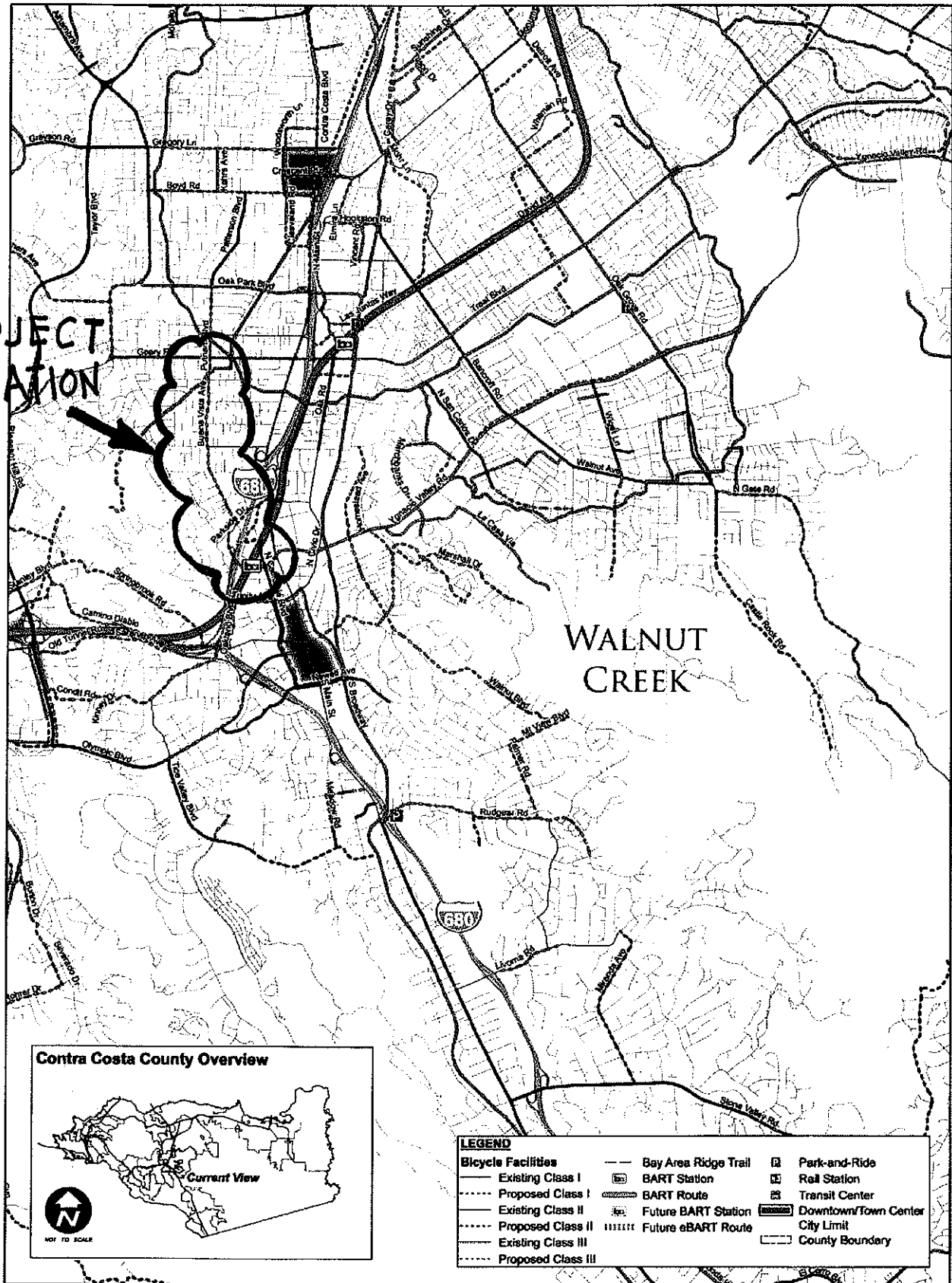


Figure 4. Bicycle Facilities (Existing and Proposed)

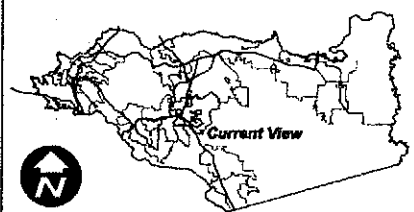
**CONTRA COSTA COUNTYWIDE BICYCLE AND
PEDESTRIAN PLAN 2009**

PROJECT
LOCATION



WALNUT
CREEK

Contra Costa County Overview



LEGEND			
Bicycle Facilities	— Bay Area Ridge Trail	□ Park-and-Ride	□ Rail Station
— Existing Class I	⊠ BART Station	⊠ Transit Center	⊠ Downtown/Town Center
⋯ Proposed Class I	⊠ BART Route	⊠ City Limit	⊠ County Boundary
⋯ Existing Class II	⊠ Future BART Station		
⋯ Proposed Class II	⊠ Future eBART Route		
⋯ Existing Class III			
⋯ Proposed Class III			

WALNUT CREEK BICYCLE MASTER PLAN 2011

APPENDIX B: NEW BICYCLE FACILITIES LIST

HIGHEST PRIORITY

The facilities listed below are considered the highest priority for funding as they have the greatest potential to provide safety and connectivity improvements and to serve the largest number of user groups as well as destinations.

Buena Vista Avenue

Type: Class III
Description: Class III from Geary Road to Parkside Drive
Length: 1.2 miles
Notes: Important facility serving NW Walnut Creek linking parks and neighborhoods to downtown; Possible Share the Road facility

California Boulevard

Type: Class II, III
Description: Class II facility on SB direction from Mt. Diablo Blvd to Olympic; Class III facility NB from Olympic to Mt. Diablo Blvd; Class III in both directions from Olympic to Newell
Length: .40 miles
Notes: Downtown gap connector, Regional gap connector, Bike Thru Lanes recommended at street intersections on both segments

Cedro Lane

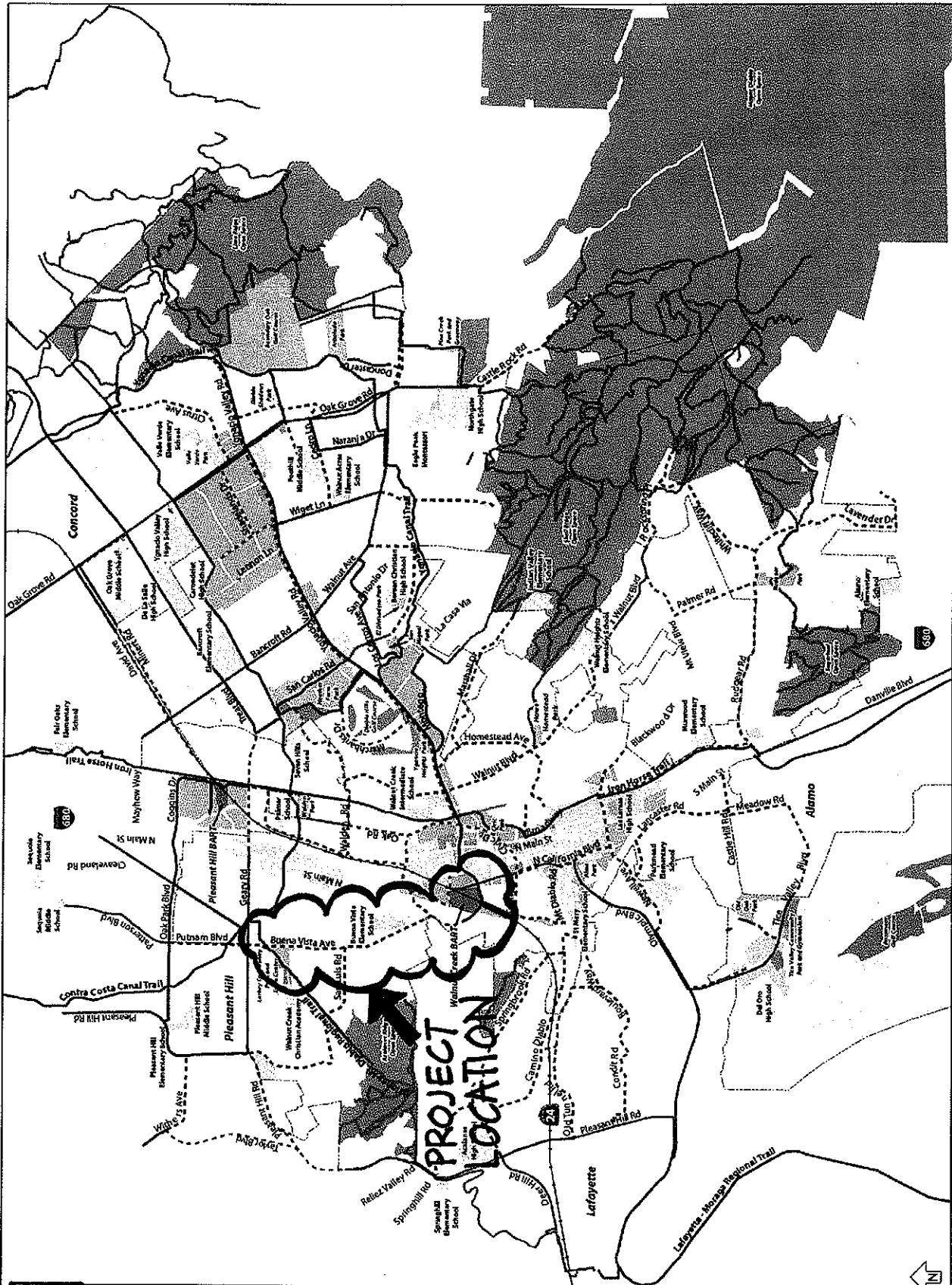
Type: Class III
Description: Class III facility between Wiget Drive to Naranja Drive
Length: .40 miles
Notes: Possible Safe Routes to School designation linking two schools

Civic Drive

Type: Class II, III*
Description: Class II from California Boulevard to Walden Road
Length: 1.1 miles total (Class II: .70 miles; Class III: .40 miles)
Notes: * Class II north of Ygnacio Valley Road; Class III with Share the Road between Ygnacio Valley Road and California Blvd.
May require removal of on-street parking spaces; Important connector to Walnut Creek BART Station important network connector to Iron Horse Trail; serves downtown area, Civic Park, City Hall

Lavender Drive

Type: Class III
Description: Class III from Rudgear Drive to Livorna Road
Length: .70 miles
Notes: Important gap connector



**City of Walnut Creek
Proposed and Existing
Bicycle Facilities**

Proposed	
-----	Class I Multi-Use Paths
-----	Class II Bicycle Lanes
-----	Class III Bicycle Routes
Existing	
-----	Class I Paved Multi-Use Paths
-----	Class I Unpaved Multi-Use Paths
-----	Class II Bicycle Lanes
-----	Class III Bicycle Routes
Points of Interest	
[Pattern]	Community Places
[Pattern]	Schools
[Pattern]	Retail & Service Areas
[Pattern]	Major Employment Centers
[Pattern]	BART Stations
[Pattern]	Park & Ride
[Pattern]	Park Facilities
[Pattern]	Open Space Recreation
[Pattern]	BART
[Pattern]	City Limit



BUENA VISTA AVE-TWO LANE ROADWAY WITH INTERRUPTED ON STREET PARKING --WITH NO OFFICIAL BIKE LANE



BUENA VISTA AVE-EXISTING BUS STOP THAT COULD BE USED WITH ALTERNATIVE USE OF TRANSPORTATION THAT CAN BE UTILIZED WITH BICYCLEWALKING



BUENA VISTA AVE-BICYCLE RIDING WITHOUT USE OF SHARROW NEAR SCHOOL CROSSWALK.



SAN JUAN AVE-STUDENTS AND PARENTS WALKING ON A NARROW WALKWAY ALONG THE FRONTAGE OF THE SCHOOL.



SAN JUAN AVE-STUDENTS AND PARENTS WALKING ON A NARROW WALKWAY ALONG THE FRONTAGE OF THE SCHOOL.



A UNIMPROVED CORNER OF SAN JUAN AVE AND ALVARADO AVE PROPOSED TO BE IMPROVED WITH ADA CURB RAMPS AND SIDEWALK.



HILLSIDE AVE LOOKING EASTBOUND UNDER 680 OVERPASS WHERE SIDEWALK IS PROPOSED WITH A PROTECTED PEDESTRIAN SIGNAL THAT WOULD CONNECT TO THE BART STATION PASS THE 680 OVERPASS.



HILLSIDE AVE LOOKING EASTBOUND UNDER 680 OVERPASS. THE SIDEWALK PROPOSED ON THE SOUTH SIDE OF THE OVERPASS WOULD PROVIDE A PROTECTED WALKWAY INCLUDING A PROTECTED PEDESTRIAN SIGNAL AT THE EXISTING TRAFFIC SIGNAL SHOWN ON THE PHOTO BELOW. THIS WALKWAY WOULD PROVIDE A CONNECTION FROM NORTHWEST WALNUT CREEK NEIGHBORHOODS TO THE BART STATION LOCATED JUST EAST OF THIS 680 OVERPASS.



HILLSIDE AVE (YGNACIO VALLEY ROAD) LOOKING WESTBOUND --SIDEWALK SHOWN BELOW TO CONNECT UNDER THE 680 OVERPASS TO A NEW WIDER SIDEWALK THAT WILL CONNECT TO THE NEW PEDESTRIAN SIGNAL TO PROVIDE A COMPLETE SIDEWALK CONNECTION UNDER THE 680 OVERPASS. THIS SIDEWALK SHOWN IN THE PHOTO BELOW LEADS TO AN EXISTING SIGNAL THAT PROVIDES A PROTECTED CROSSING TO THE BART STATION.



Buena Vista School

2355 San Juan Avenue * Walnut Creek * CA 94597 * Tel (925) 944-6822 * Fax (925) 934-8907

*Contra Costa Transportation Authority
Brad Beck
2999 Oak Rd, Suite 100
Walnut Creek, CA 94597*

Dear Mr. Beck,

I am writing to express Buena Vista School's and our School Community's support for the City of Walnut Creek's grant application for capital funds to implement the Northwest Walnut Creek Pedestrian and Bicycle Access Improvements Plan. This project will provide pedestrian and/or bicycle improvements for Buena Vista Ave, Parkside Drive, Hillside Ave, Alvarado Blvd and San Juan Ave. These connections will help to encourage pedestrian and bicycle travel throughout the neighborhood and provide access to not only our facility but other neighboring facilities as well.

This project will provide several physical improvements to enhance pedestrian and bicycle travel through the neighborhood. New sharrows will provide a designated route for bicyclists, while sidewalk improvements, a pedestrian signal and new paths will create additional connections to BART, the school and other places in our neighborhood.

I encourage you to fund this project and support access to the Buena Vista School Community and the surrounding community. If you have any questions please contact Mrs. Cindy Jaconette, 925-944-6822 x 3004.

Sincerely,

Cindy Jaconette

*Mrs. Cindy Jaconette
Principal, Buena Vista School
2355 San Juan Ave
Walnut Creek, Ca. 94597
925-944-6822 x 3004*



康郡福音教會

Contra Costa Gospel Church

2460 Buena Vista Ave. Walnut Creek, CA 94597

Tel: (925) 933-0112 Fax: (925) 933-1441

Yun Na Rhee
City of Walnut Creek
Assistant Engineer
(925) 256-3529 ext.2219
<rhee@walnut-creek.org>
Subject: Transportation for Livable Communities Grant

November 5, 2011

Dear Ms. Rhee,

I am writing to express the support of Contra Costa Gospel Church for the City of Walnut Creek's grant application for capital funds to implement the Northwest Walnut Creek Pedestrian and Bicycle Access Improvements Plan. This project will provide pedestrian and/or bicycle improvements for Buena Vista Ave, Parkside Drive, Hillside Ave, Alvarado Blvd and San Juan Ave. These connections will help to encourage pedestrian and bicycle travel throughout the neighborhood and provide access to not only our facility but other neighboring facilities as well.

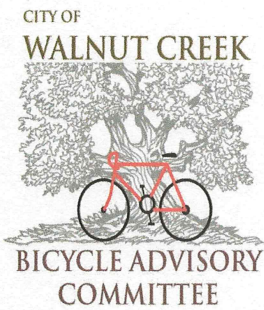
This project will provide several physical improvements to enhance pedestrian and bicycle travel through the neighborhood. It will provide a designated route for bicyclists, while sidewalk improvements, a pedestrian signal and new paths will create additional connections to BART, the school and other places in our neighborhood.

I encourage you to fund this project and support access to contra Costa Gospel Church and the surrounding community. If you have any questions please contact me at 925-423-0762.

Sincerely,

David Chin W. Ma

President, Contra Costa Gospel Church



November 10, 2011

Mr. Brad Beck
Senior Transportation Planner
Contra Costa Transportation Authority
2999 Oak Road, Suite 100
Walnut Creek CA 94597

Dear Mr. Beck,

I am writing to express the Walnut Creek Bicycle Advisory Committee's support for the City of Walnut Creek's grant application for capital funds to implement the Northwest Walnut Creek Pedestrian and Bicycle Access Improvements Plan.

In 2011, the City of Walnut Creek adopted its Bicycle Master Plan to establish a framework and support future development of bicycle facilities. The proposed improvements associated with this project are consistent with the recently adopted Plan.

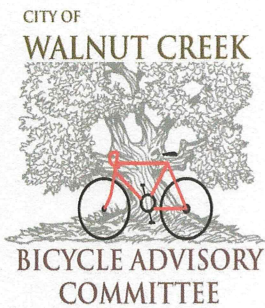
Specifically, the project will provide pedestrian and/or bicycle improvements for Buena Vista Ave, Parkside Drive, Hillside Ave, Alvarado Blvd and San Juan Ave. All of these facilities are adjacent to schools, churches and residents. Funding these improvements will help to encourage pedestrian and bicycle travel throughout the neighborhood and provide access to not only our facilities but other neighboring facilities as well. The Bicycle Advisory Committee believes this project is ideal for the Transportation for Livable Communities grant, as it meets or exceeds many of the policies and goals within the TLC program.

The Walnut Creek BAC encourages Contra Costa Transportation Authority to fund this project and support the safety and access improvements this project will provide to children, adults and seniors in Walnut Creek and the surrounding communities.

Sincerely,



Jim Vendel
Chairperson
Walnut Creek Bicycle Advisory Committee



November 10, 2011

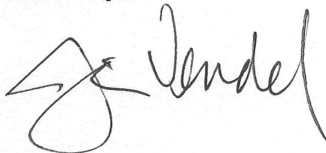
Mr. Brad Beck
Senior Transportation Planner
Contra Costa Transportation Authority
2999 Oak Road, Suite 100
Walnut Creek CA 94597

Dear Mr. Beck,

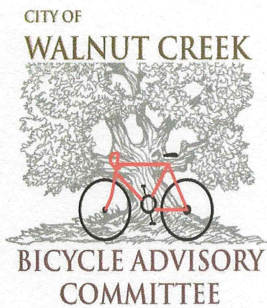
The Walnut Creek Bicycle Advisory Committee (BAC) fully supports Contra Costa County's Transportation for Livable Communities (TLC) grant application for the Treat Boulevard and Interstate 680 planning grant to study options that improve the bicycle/pedestrian connectivity between the Pleasant Hill BART Station and North Main Street in Walnut Creek.

Treat Boulevard is a roadway identified in the City of Walnut Creek's Bicycle Master Plan, the Countywide Bicycle and Pedestrian Master Plan, and the Contra Costa Centre Specific Plan for improved access to the Pleasant Hill BART Station. Currently pedestrians and bicyclists alike have difficulty using the I-680 over-crossing, as the existing bridge width and lack of sidewalks limit opportunities for safe access between the residential and commercial areas in Walnut Creek and the BART Station. The Walnut Creek Bicycle Advisory Committee believes this planning study is necessary to explore the various engineering and funding options that need to be considered in order to provide both pedestrians and bicyclists with safe mobility and access across the I-680 freeway.

Sincerely,



Jim Vendel
Chairperson
Walnut Creek Bicycle Advisory Committee



November 10, 2011

Mr. Brad Beck
Senior Transportation Planner
Contra Costa Transportation Authority
2999 Oak Road, Suite 100
Walnut Creek CA 94597

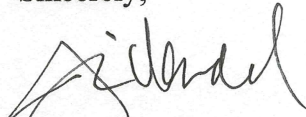
Dear Mr. Beck,

The Walnut Creek Bicycle Advisory Committee (BAC) fully supports the City of Walnut Creek's Transportation for Livable Communities (TLC) grant application for the completion of a Pedestrian and Complete Streets Master Plan for Walnut Creek.

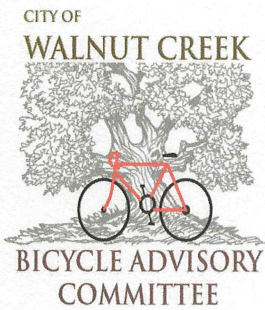
Since its inception, the Walnut Creek BAC has promoted the construction of new pedestrian and bicycle facilities throughout Walnut Creek. The BAC is committed to the growing demand for safe pedestrian and bicycle transportation in Walnut Creek. As you know, fuel prices will continue to rise or remain unpredictable, and people continue to look for alternatives to the automobile. We also believe that a Pedestrian Master Plan will address concerns about the growing obesity epidemic, improve air quality and contribute to an enhanced quality of life.

In 2011, the City of Walnut Creek adopted its Bicycle Master Plan to establish a framework and support future development of bicycle facilities. The BAC believes a Pedestrian Master Plan with Complete Street policies will establish a similar framework for development and encourage the routine accommodation of all transit modes. The Committee would like to express its support of Walnut Creek's TLC application for the creation of a Pedestrian Master Plan and recommends Contra Costa Transportation Authority approve grant funding.

Sincerely,



Jim Vendel
Chairperson
Walnut Creek Bicycle Advisory Committee



November 10, 2011

Mr. Brad Beck
Senior Transportation Planner
Contra Costa Transportation Authority
2999 Oak Road, Suite 100
Walnut Creek CA 94597

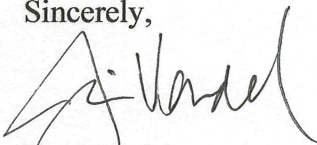
Dear Mr. Beck,

The Walnut Creek Bicycle Advisory Committee (BAC) fully supports Contra Costa County's Transportation for Livable Communities (TLC) grant application for the Olympic Corridor Trail Connector planning grant to study options that improve the bicycle/pedestrian connectivity between the Lafayette Moraga Regional Trail and the Iron Horse Regional Trail in downtown Walnut Creek.

The Olympic Corridor has been included as a top priority in the City of Walnut Creek's Bicycle Master Plan, the Countywide Bicycle and Pedestrian Master Plan, and the City of Lafayette Bicycle Plan for improved access to downtown Walnut Creek, including the Walnut Creek BART Station and for providing options to close a significant facility gap between these highly important and heavily used regional trails.

Additionally, the Bicycle Advisory Committee has identified improved access to downtown Walnut Creek as top priority and believes this grant will allow the City of Walnut Creek, in conjunction with Contra Costa County, to proactively address the ongoing need for strategic transportation planning based on the need to reduce single occupancy vehicle trips and improve alternative access to multi-modal transit stops. We look forward to working with Contra Costa County and the City of Walnut Creek's staff through the public review process and recommend the grant application for the Olympic Corridor Trail Connector be awarded.

Sincerely,



Jim Vendel
Chairperson
Walnut Creek Bicycle Advisory Committee



SAN FRANCISCO BAY AREA RAPID TRANSIT DISTRICT

300 Lakeside Drive, P.O. Box 12688
Oakland, CA 94604-2688
(510) 464-6000

2011

October 25, 2011

Bob Franklin
PRESIDENT

John McPartland
VICE PRESIDENT

Grace Crunican
GENERAL MANAGER

Mr. Brad Beck, Senior Transportation Planner
Contra Costa Transportation Authority
2999 Oak Rd, Suite 100
Walnut Creek, CA 94597

Re: Support for the Northwest Walnut Creek Pedestrian and
Bicycle Access Plan Grant Proposal

DIRECTORS

Gail Murray
1ST DISTRICT

Joel Keller
2ND DISTRICT

Bob Franklin
3RD DISTRICT

Robert Raburn
4TH DISTRICT

John McPartland
5TH DISTRICT

Thomas M. Blalock, P.E.
6TH DISTRICT

Lynette Sweet
7TH DISTRICT

James Fang
8TH DISTRICT

Tom Radulovich
9TH DISTRICT

Dear Mr. Beck:

On behalf of the Bay Area Rapid Transit (BART) District, I am writing to support the City of Walnut Creek's grant application for capital funds to implement the Northwest Walnut Creek Pedestrian and Bicycle Access Improvement Plan. This project will greatly improve pedestrian and bicycle access to and between two BART stations in the City. In addition, this project directly supports the development of the transit village at the Walnut Creek BART station by providing direct access to the station by modes other than the automobile.

The Northwest Walnut Creek Pedestrian and Bicycle Access project will greatly encourage both commuting and recreational walking and bicycling from and within several neighborhoods in Walnut Creek that are somewhat isolated from the BART stations and downtown. This project will provide pedestrian and/or bicycle improvements for Buena Vista Ave, Parkside Drive, Hillside Ave, Alvarado Blvd and San Juan Ave.

This project will provide critical physical improvements to enhance pedestrian and bicycle travel through the neighborhood. New sharrows will provide a designated route for bicyclists, while sidewalk improvements, a pedestrian signal and new paths will create additional connections to BART, the local elementary school and other places in our neighborhood.

We support this project and look forward to working with the City of Walnut Creek on this and other access projects to BART. If you have any questions or concerns please contact Deidre Heitman, Principal Planner, at (510) 287-4796.

Sincerely,

Carter Mau, Executive Manager
Office of Planning and Budget



Application Form

Measure J Transportation for Livable Communities (CC-TLC) Program Funds

1. PROJECT INFORMATION

Project Name Olympic Corridor Trail Connector Study

Project Location The project spans three jurisdictions generally along the Olympic Boulevard corridor. The western project limit is in the City of Lafayette at the intersection of the Lafayette-Moraga Trail, Olympic Boulevard, and Reliez Station Road. The eastern project limit is in the City of Walnut Creek along the Iron Horse Trail corridor. Mid-corridor is in unincorporated Contra Costa County.

Sponsor Contra Costa County

Implementing agency Contra Costa County

Partner agencies City of Lafayette, City of Walnut Creek

Contact for project John Cunningham, Sr. Transportation Planner, 925-335-1243, john.cunningham@dcd.cccounty.us

2. FUNDING

In thousands of dollars, please provide the estimated cost of the proposed project, the amount of CC-TLC funds requested, and any other funds that have been committed to the project.

<i>Source</i>	<i>Amount</i>
Total project cost †	\$195,000
Requested CC-TLC funds	\$195,000
Other committed funding 1 ‡	\$000
Other committed funding 2 ‡	\$000
Unfunded balance	\$195,000

† Minimum request is \$50,000

‡ Add other committed funding sources as needed

3. POTENTIAL PHASING

{Identify project components that could be eliminated if insufficient funding is available to fully fund the project as proposed and the estimated reduction in project cost and requested Measure J funds}

The proposal only proposes completion of the planning phase. It does not include project design or environmental review. We cannot propose partial planning for this project. The planning will be done together.

Due to likely differences in each jurisdiction in terms of 1) identified improvements, 2) funding readiness, and 3) project advancement protocols, final project implementation must be done individually as part of a future process.

4. PROJECT DESCRIPTION

{In a single paragraph, provide a short description of the project and its purpose, location and key components}

The Olympic Corridor Trail Connector Study (Study) will identify feasible alignments, a preferred alternative alignment, and design elements for a pedestrian/bicycle facility connecting two well used paved multi-use regional trails in Contra Costa County, the Lafayette-Moraga Trail (LMT) and the Iron Horse Trail (IHT). Specifically, the study will recommend a range of improvements intended to be implemented in phases. Recommendations ranging from short-term/low cost spot improvements to longer term/substantial investments that reallocate existing right-of-way or acquire new right-of-way will be provided. These improvements are intended to develop the ultimate design of a LMT and IHT connector facility which provides the highest level of safety and convenience for pedestrians and cyclists along the corridor. The provision of an improved connector facility will encourage more walking and bicycling along the corridor being studied.

{Describe the proposed project more fully including the project purpose, its features, its location and the how the sponsoring or implementing agency intends to operate and maintain the project. At a minimum, attach a location map. Photos and designs may be included as well.}

The Study will examine, in detail, the Olympic Boulevard Corridor from the termination of the LMT at Reliez Station Road/Olympic Boulevard in the west (in the City of Lafayette), along Olympic Boulevard (Unincorporated Contra Costa County) to the IHT in the eastern end of the corridor (City of Walnut Creek).

Potential alignments to be studied include Olympic Boulevard, Boulevard Way, and Newell Avenue. In addition to these roads, the study will also examine publically owned

parcels (Flood Control, Caltrans) to identify opportunities for more direct connections and/or separated, Class I type facilities.

As indicated above, the goal is to provide a connection that is as similar as possible to the IHT and LMT facilities, that is to say a separated facility providing the maximum amount of comfort to non-motorized travelers. Providing a facility of this type is justified in that, 1) the LMT and the IHT are two of the most well used trails in Contra Costa County, and 2) the land uses anchoring either end of the corridor further point to the need for a superior connection. In the west there are substantial residential trip origins and the City of Lafayette's commercial district along Mt. Diablo Boulevard. In the east there is an intense retail commercial area (Broadway Plaza) that is one of the County's top retail and entertainment destinations. Plans to redevelop this area are in process, this process will inform that effort according to City of Walnut Creek staff. Mid-corridor there is a substantial number of higher density residential units that are potential users of the trail.

An early review of potential alignments suggests that the facility will ultimately be located in right-of-way currently under use and maintenance. That said, any new facility will fall under the operations/maintenance the existing infrastructure is covered by. In the event new right-of-way is used for an alignment (Flood Control property for example) and operations and maintenance plan will be included as a part of the study recommendations.

Please see the next page for a study location map.

5. PROPOSED SCHEDULE

In the following table, enter the expected beginning and ending *month and year* for each of the phases listed. If the phase has been finished, enter "Completed" in the "To" column.

<i>Phase</i>	<i>From</i>	<i>To</i>
Preliminary Design & Planning		
Design		
Environmental Clearance		
Right of Way		
Construction		

The schedule above contains steps describing a construction project and does not lend itself to describing a planning process. The following is the proposed schedule for the subject study:

<i>Phase</i>	<i>From</i>	<i>To</i>
Form Project Steering Committee	May 2012	June 2012
Project Initiation (meet w/consultant, finalize study process, site visit, develop inventory sufficient for first public meeting)	June 2012	July 2012
Public Meeting #1: Review existing conditions, collect input re: concerns, vision, goals.	August 2012	August 2012
DRAFT Alternative Alignments/Issues & Options & Review by County/City Staff(based on initial inventory work by consultant & input from public)	September 2012	October 2012
Public Meeting #2:(Review/comment draft alternatives)	November 2012	November 2012
Technical Memo Identifying Preferred Alignment (review input from public w/County/City staff)	November 2012	December 2012
Consultant Develops Preferred Alignment Detail in consultation with County/City staff(design features, alignment conflicts preliminary engineering, phasing, cost estimate)	January 2012	February 2012
Public Meeting #3: Review Preferred Alignment & Design	March 2012	March 2012
Consultant Finalizes Recommendations (each implementing jurisdiction will have separate tasks/phasing)	April 2012	May 2012

6. ABILITY TO MEET CRITERIA

Describe how the proposed project would meet the following four criteria:

Achievement of CC-TLC

Goals: Identify which of the six goals of the CC-TLC program the proposed project would help achieve – a project need not meet all of them – and *briefly* describe how it would do so (see Exhibit One for these six goals)

Help create walkable, pedestrian-friendly neighborhoods and business districts

The goal of the proposal is to provide a superior connection for bicyclists and pedestrians between the Lafayette-Moraga Trail to the Iron Horse Trail. The preferred design recommendations are likely to include separate facilities for bicycle and pedestrian use at certain points within the corridor. It is well documented that superior facilities encourage bicycle/pedestrian travel.

ii. Promote innovative solutions, including compact building design and context-sensitive site planning that is integrated with the transportation system

The project will consider innovative solutions to non-motorized travel including, but not limited to best practices found in the “Other Tools For Local Agencies” in the 2009 Contra Costa Countywide Bicycle and Pedestrian plan, the new National Association of City Transportation Officials (NACTO) Urban Bikeway Design Guide, the Contra Costa Trail Design Resource Handbook.

iii. Help create walkable, pedestrian-friendly access linking housing and job centers to transit

Consistent with the Transportation for Livable Communities program description in the voter approved Measure J, this project will help to create an improved walkable and bikeable access to housing, job centers and transit by:

1. Providing a high quality non-motorized facility(s) connecting the housing and commercial/retail areas (Mt. Diablo Blvd) in Lafayette with the retail, commercial and job
-

center in Walnut Creek,

2. Providing an improved bicycle and pedestrian connection from Lafayette and unincorporated County (that has very limited transit service) to the transit rich areas of downtown Walnut Creek. 13 County Connection routes stop in and around the Walnut Creek BART station. Solano Express and the Livermore Amador Valley Transit Authority also have routes that serve the station. In contrast the next nearest BART station (Lafayette) is served by only one transit provider (County Connection) with only two routes. There is very limited transit service in the corridor being studied, and

3. Improving access to the Iron Horse Trail which, in turn, provides additional non-motorized, countywide access to retail, commercial and job centers.

iv. Help create affordable housing

The project will improve non-motorized connections to job centers in Walnut Creek (downtown) and Lafayette (Mt. Diablo Blvd.) to multi-family housing present along Boulevard Way and Olympic Boulevard.

Feasibility: Describe why the project is feasible and any outstanding issues in the project development process – design, environmental clearance, right-of-way purchase, and PS&E – or funding of the project

There are no outstanding issues with the conduct of this study. As indicated below, there is substantial policy support for the proposed project. The project is a planning study so flaws or issues that appear during construction are not likely to appear. One purpose of the study is to *identify* any issues or flaws.

Local and policy support: Identify local policies that support the project and the integration of the project with other local efforts, as well as other support from the general public, the RTPCs and other relevant agencies

The study corridor and potential alignments within are well supported in numerous local, countywide, and regional planning documents including the Contra Costa Countywide Bicycle and Pedestrian Plan (2009), The City of Walnut Creek Bicycle Plan (2011), DRAFT Contra Costa County General Plan/Bicycle Facilities Map (2012), MTC Regional

Bicycle Plan (2009 Update), and the City of Lafayette Bikeways Masterplan (2006). Please see *Attachment 1: Policy Support for the Olympic Corridor Trail Connector Study* for details on policy support.

Matching funds: Identify any funds from other sources that are or would be committed to the project

{What percentage of non-Measure J funds would be committed to the project?}

The County has not proposed a source of matching funds for the proposed planning study.

EXHIBIT ONE

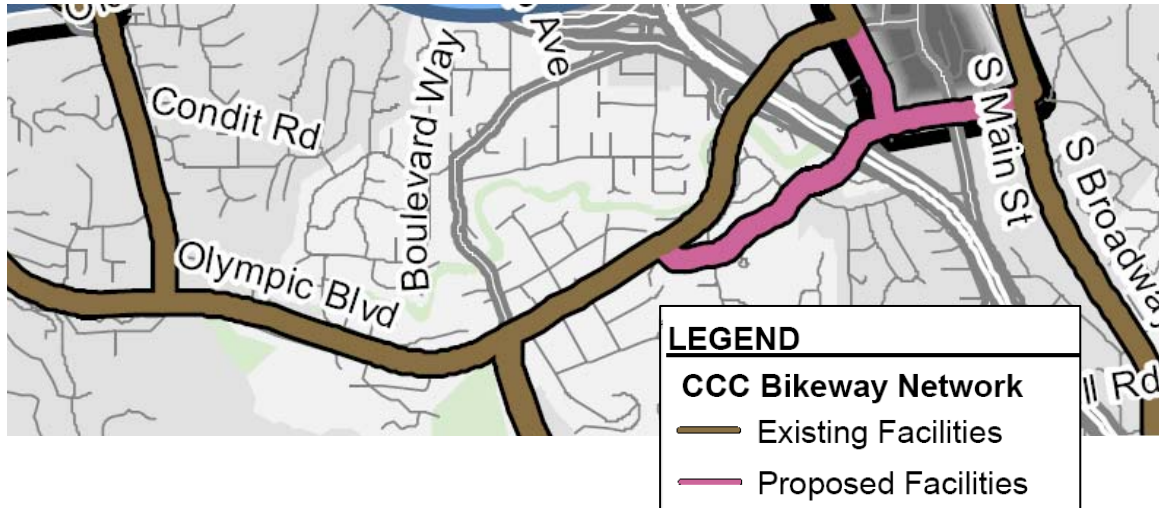
Goals of the Measure J CC-TLC Program

Measure J establishes six goals for the Measure J Transportation for Livable Communities program.

- Help create walkable, pedestrian-friendly neighborhoods and business districts
- Promote innovative solutions, including compact building design and context-sensitive site planning that is integrated with the transportation system
- Help create walkable, pedestrian-friendly access linking housing and job centers to transit
- Help create affordable housing
- Encourage a mixture of land uses and support a community's development or redevelopment activities
- Provide for a variety of transportation choices to enhance a community's mobility, identity, and quality of life

Attachment 1: Policy Support for the Olympic Corridor Trail Connector Study

Contra Costa Transportation Authority: 2009 Contra Costa Countywide Bicycle and Pedestrian Plan: Figure 1: Countywide Bicycle Network: The figure below shows an existing facility along Olympic (partially in error, see conflicting designation in Figure 2 below. This error will be rectified with the upcoming Countywide Bicycle Plan facility map update), and a proposed facility on Newell Avenue.



Contra Costa Transportation Authority: 2009 Contra Costa Countywide Bicycle and Pedestrian Plan: Figure D-19: Local Bicycle Networks: Walnut Creek: This figure shows proposed facilities on Olympic Boulevard and Boulevard Way and an existing Class II on Newell (The existing Class II is in error [see conflicting designation in Figure 1 above], there should be a proposed Class III. This error will be rectified with the upcoming Countywide Bicycle Plan facility map update).

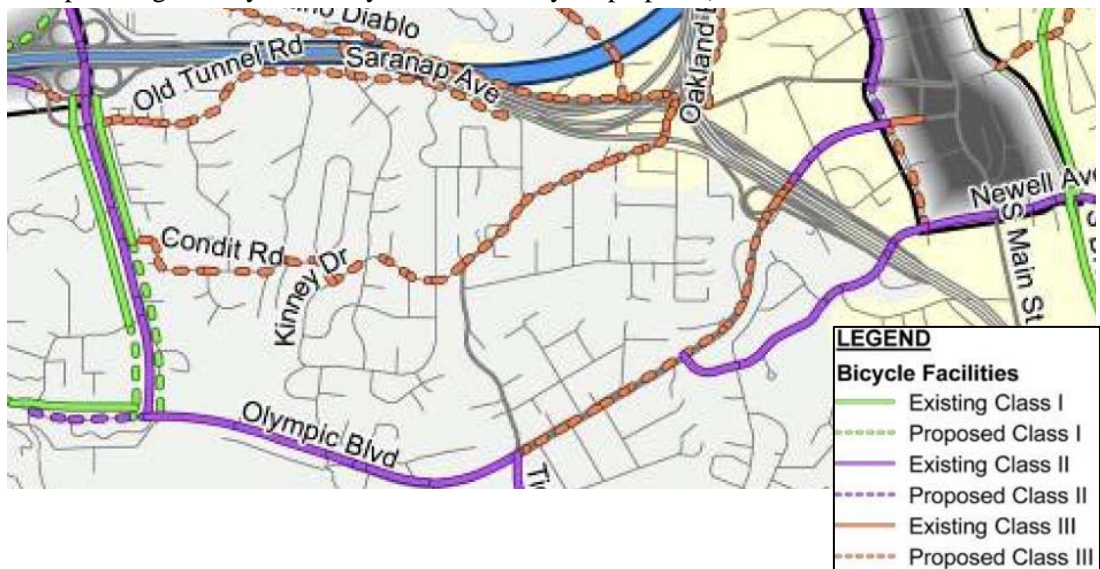


Figure 3: City of Walnut Creek, Proposed and Existing Bicycle Facilities Map: This figure shows proposed facilities along Newell Avenue, Lilac Avenue, and Boulevard Way, all included in the study scope.

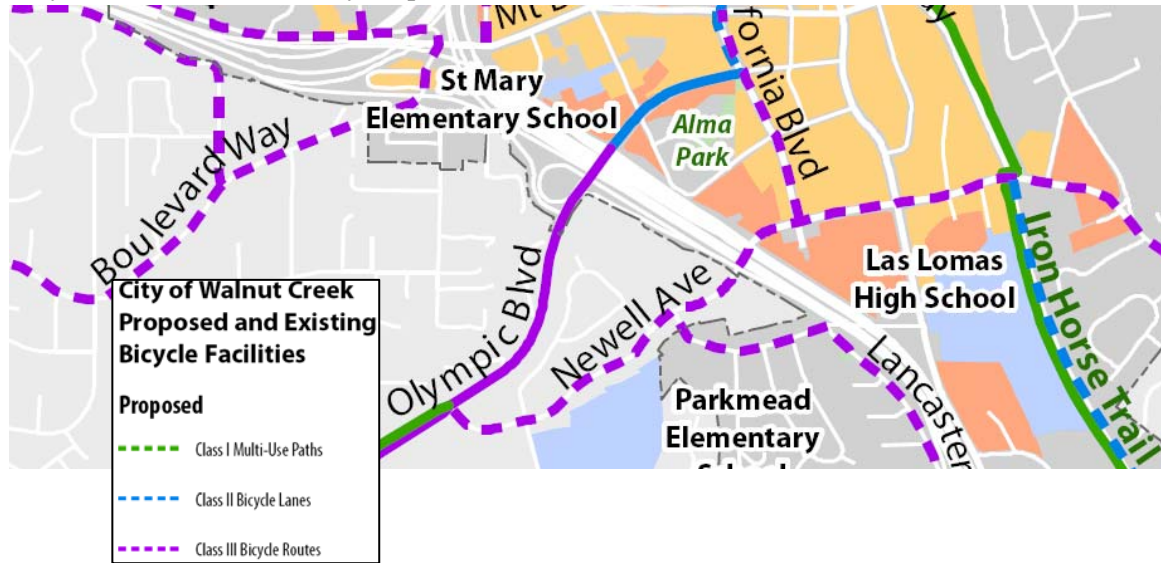


Figure 4: DRAFT Contra Costa County General Plan: Figure XX: DRAFT Updated Planning and Existing Bicycle Facilities: This figure shows planning facilities along Newell Avenue, Boulevard Way, and Olympic Boulevard.

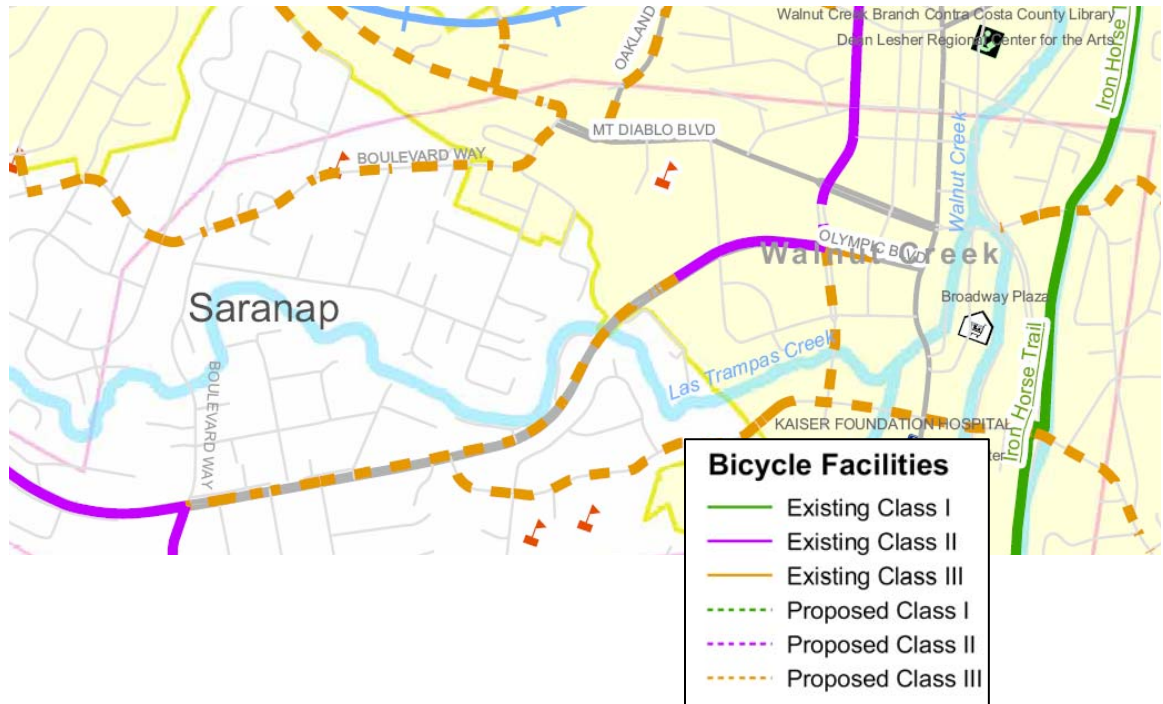
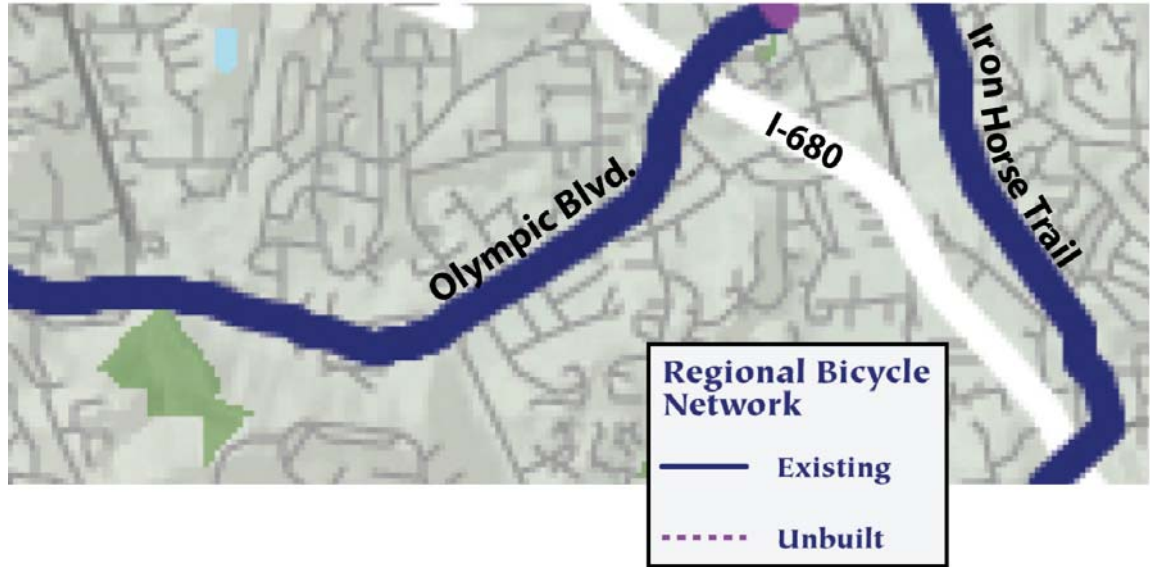


Figure 5: Metropolitan Transportation Commission: Regional Bicycle Plan: Contra Costa County Bikeway Network: This figure shows, incorrectly, an existing facility along Olympic Boulevard.



November 14, 2011

Brad Beck
Senior Transportation Planner
Contra Costa Transportation Authority
2999 Oak Road, Suite 100
Walnut Creek, CA 94597

RE: CC-TLC Grant Applications

Dear Mr. Beck:

Please find attached applications for the Measure J Transportation for Livable Communities Grant Program. We are submitting applications for each of three RTPC areas as follows:

- San Pablo Dam Road Walkability and Community Enhancement Project (WCCTAC)
- Pacheco Boulevard Bike and Pedestrian Project (TRANSPAC)
- Stone Valley Road Bike Lane Project (SWAT)

If you need additional information or have any questions regarding the applications or projects, please contact me at (925) 313-2327.

Sincerely,



Mary Halle
Associate Civil Engineer
Transportation Engineering

MH:ms
g:\G:\transeng\2011\TE\TLCTransmittal Letter 11-14-11.doc
Enclosures

c: J. Bueren, Director
S. Kowalewski, Deputy Director
J. Fahy, Transportation Engineering



Application Form

Measure J Transportation for Livable Communities (CC-TLC) Program Funds

1. PROJECT INFORMATION

Project Name..... PACHECO BLVD BIKE AND PEDESTRIAN PROJECT

Project Location..... North side of Pacheco Boulevard, Pacheco; from Windhover Way to 230 feet south of Morello Avenue.

Sponsor..... Contra Costa County Public Works

Implementing agency {if different than sponsor}

Partner agencies {list only those agencies whose role is essential in implementing the proposed project}

Contact for project ... Rene Urbina, Staff Engineer
 (925) 313-2308
rurbi@pw.cccounty.us
 255 Glacier Drive, Martinez, CA 94553

2. FUNDING

In thousands of dollars, please provide the estimated cost of the proposed project, the amount of CC-TLC funds requested, and any other funds that have been committed to the project.

<i>Source</i>	<i>Amount</i>
Total project cost †	\$1,150,000
Requested CC-TLC funds	\$850,000
TDA	\$100,000
Martinez Area of Benefit Funds	\$200,000
Unfunded balance	\$0,000

† Minimum request is \$50,000

‡ Add other committed funding sources as needed

3. POTENTIAL PHASING

{Identify project components that could be eliminated if insufficient funding is available to fully fund the project as proposed and the estimated reduction in project cost and requested Measure J funds}

County staff is currently designing the Pacheco Boulevard Bike and Pedestrian Project from Windhover Way to Goree Court. Approximately \$300,000 is already committed for this project. If awarded, the TLC grant funds would fully fund this project and allow the project to be extended to 230 feet south of Morello Avenue to bridge the final gap of 1,200 feet of bike lane and sidewalk. The result will be continuous sidewalk and bike lane for 10,300 feet that would extend from I-680 to Bush Street in Martinez. If insufficient funds are available for the extent of the proposed project, the limits could be reduced to the original distance from Windhover Way to Goree Court and efforts would continue in the following years to construct the remaining 700 feet south of Goree Court. With the scope reduction, Measure J funds requested for the project could be reduced from \$850,000 to \$500,000.

4. PROJECT DESCRIPTION

{In a single paragraph, provide a short description of the project and its purpose, location and key components}

This project will include construction of 1,200 feet of sidewalk and bike lane on the north side of Pacheco Boulevard, from Windhover Way to 230 feet south of Morello Avenue to provide a pedestrian friendly link for residents who walk or bike to employment centers, schools and businesses along Pacheco Boulevard. This project will remove a barrier for pedestrians, provide transportation choices, and close the gap in the pedestrian path extending from I-680 to the Shell Refinery. Please see the attached photos which identify the existing pedestrian barrier.

{Describe the proposed project more fully including the project purpose, its features, its location and the how the sponsoring or implementing agency intends to operate and maintain the project. At a minimum, attach a location map. Photos and designs may be included as well.}

This project will construct approximately 1,200 linear feet of 6.5' wide concrete sidewalk with curb and gutter and a 5' wide class II bike lane from Windhover Way to 230 south of Morello Avenue. Driveway conformances will be installed as required. The project will include ADA compliant curb ramps to be installed at the corners of Windhover Way and Goree Ct.

In front of one parcel, located between Windhover Way and Goree Court, a large earth mound will be removed to accommodate sidewalk, curb and gutter. A wood retaining wall will be constructed along the frontage of this area to hold the slope, and related conform grading performed. The design will also remove and replace sections of an existing HMA driveway to maintain access to several residences.

Bike lanes will be provided along the length of the project to finish the bikeway on the north side of Pacheco Boulevard, which is consistent with the Countywide Bike and Pedestrian Plan.

This project will require minimal right of way acquisition to place the sidewalk at its ultimate location. Since the existing right of way on Pacheco Boulevard is very irregular, a few strips of right of way acquisition will be necessary for this project. The right of way to be acquired varies from 1' to 6' wide in front of the properties, and will not require demolition of existing structures. Utility poles will need to be relocated prior to construction of this project.

In front of a parcel, located about 200 feet north of Goree Court, a second earthen mound is capped with asphalt. It will be removed to accommodate sidewalk, curb and gutter. Since the parking lot on this parcel is about 2.5' higher than the future sidewalk, a short masonry retaining wall will be installed along the property line. Existing metal gate and existing asphalt driveway will be modified to conform to the grades of the new sidewalk.

At the west end of the project, the design will include a storm drain inlet and an 18" storm drain lateral that will connect to an existing 48" storm drain line.

Pacheco Boulevard is an area in need of pedestrian and bike lane improvements due to the large amount of traffic (average daily trips of 18,519 vehicles per day) and the discontinuity of sidewalk and bike lanes. A segment of sidewalk improvements on Pacheco Boulevard was completed a few years ago near Las Juntas Elementary School. Another segment of sidewalk improvements on Pacheco Boulevard east of Windhover Way is currently in the design phase and scheduled for construction in 2012. This candidate project would be the section that will close the last gap of sidewalk and bike lane to complete a walkable, pedestrian-friendly road for Pacheco Boulevard, where residents can walk or bike to employment centers, local business, schools and transit.

Contra Costa County will maintain these proposed facilities in the future for perpetuity, and it includes pavement, curb and gutter, curb ramps, sidewalks and retaining walls among other things.

5. PROPOSED SCHEDULE

In the following table, enter the expected beginning and ending *month and year* for each of the phases listed. If the phase has been finished, enter "Completed" in the "To" column.

<i>Phase</i>	<i>From</i>	<i>To</i>
Preliminary Design & Planning	January 2011	Completed
Design	October 2011	July 2012
Environmental Clearance	October 2009	Completed
Right-of-Way	July 2012	January 2013
Construction	June 2013	August 2013

6. ABILITY TO MEET CRITERIA

Describe how the proposed project would meet the following four criteria:

Achievement of CC-TLC Goals:

Identify which of the six goals of the CC-TLC program the proposed project would help achieve — a project need not meet all of them — and *briefly* describe how it would do so (see Exhibit One for these six goals)

Feasibility: Describe why the project is feasible and any outstanding issues in the project development process — design, environmental clearance, right-of-way purchase, and PS&E — or funding of the project

Local and policy support: Identify local policies that support the project and the integration of the project with other local efforts, as well as other support from the general public, the RTPCs and other relevant agencies

Matching funds: Identify any funds from other sources that are or would be committed to the project

{What percentage of non-Measure J funds would be committed to the project?}

Achievement: This project will meet 3 of the goals of the CC-TLC program; one is that this project will help create walkable, pedestrian-friendly neighborhoods and business districts. Pacheco Boulevard is a busy road with average daily trips (ADT) of 18,519.

Installation of a continuous sidewalk and bicycle infrastructure will eliminate safety concerns and encourage residents to choose alternative modes of transportation. This project is part of a larger goal which consists of closing all the gaps of sidewalk and bike lanes on the north side of Pacheco Boulevard.

The second TLC goal this project satisfies is to create walkable, pedestrian-friendly access linking housing and job centers to transit. The area is bordered to the north, west and south by the City of Martinez and to the east by Interstate 680. The area is suburban and surrounding the project is a mixed use of residential, commercial and industrial businesses. A transitional family shelter is located near the project site, and the project will provide these children a safe path to school. Having sidewalk and bike lane infrastructure in place will link residents of the area with employment centers along Pacheco Boulevard in The Vine Hill area and in the City of Martinez.

The third goal this project meets is that the project provides for a variety of transportation choices to enhance a community's mobility, identity, and quality of life. By improving the sidewalk and bike lane infrastructure of this area, this project provides the residents with viable means of mobilization as it connects to local bus service (County Connection) which links to Amtrak and BART. For instance, pedestrians would be able to walk on a continuous sidewalk on the north side of Pacheco Boulevard from their residence to the bus stop at Morello Avenue and Pacheco Boulevard, where they could take a bus (Route 316) to connect to the Amtrak Martinez train station in downtown Martinez, or the Pleasant Hill BART Station in Pleasant Hill to commute to work or schools, including connecting to the local community college (Diablo Valley Community College in Pleasant Hill).

Feasibility: The Pacheco Boulevard Bicycle and Pedestrian Project is an active project that has completed the planning phase and is currently under design. Staff has worked over the past ten years to gradually close the gap and create a pedestrian friendly environment for the Vine Hill area that combines residential, industrial and commercial areas, with focal points at the Las Juntas Elementary School, Transitional Family Shelter and the major employment center at the Shell Refinery. The first phase of the project was completed in 2002, the second phase will be constructed the summer of 2012, and the final phase is proposed under this grant program. The CEQA documentation has been completed for a portion of the proposed project limits from Windhover Way to Goree Court. Grant funding would allow the project to extend the final 700 feet to complete the pedestrian path. The project currently has a commitment of \$300,000 in local funds and TDA funding. The project requires minimal right of way acquisition, and temporary construction easements for conform work. The project is supported by the local school, Supervisor for District V, and CHP. Staff does not anticipate any outstanding issues.

Local and policy support: This project was formulated by the Contra Costa County Public Works Department, Transportation Engineering division after identifying the lack

of sidewalk and bike lane infrastructure as a barrier to pedestrian travel. The principal of Las Juntas Elementary School is in support of this project, since many of the students walk or bike to school. The Martinez Unified School District supports sidewalk projects on Pacheco Boulevard to improve safety for its students. The Contra Costa County District V Supervisor and CHP indicated their support for this sidewalk and bike lane project as well. A bike lane along Pacheco Boulevard is in compliance with the Countywide Bike and Pedestrian Plan.

Matching Funds: Staff has secured funding from a Transportation Development Act (TDA) grant for \$100,000 and allocated local funds of \$200,000. Therefore, approximately 26% of the project would be funded with non-Measure J funds.

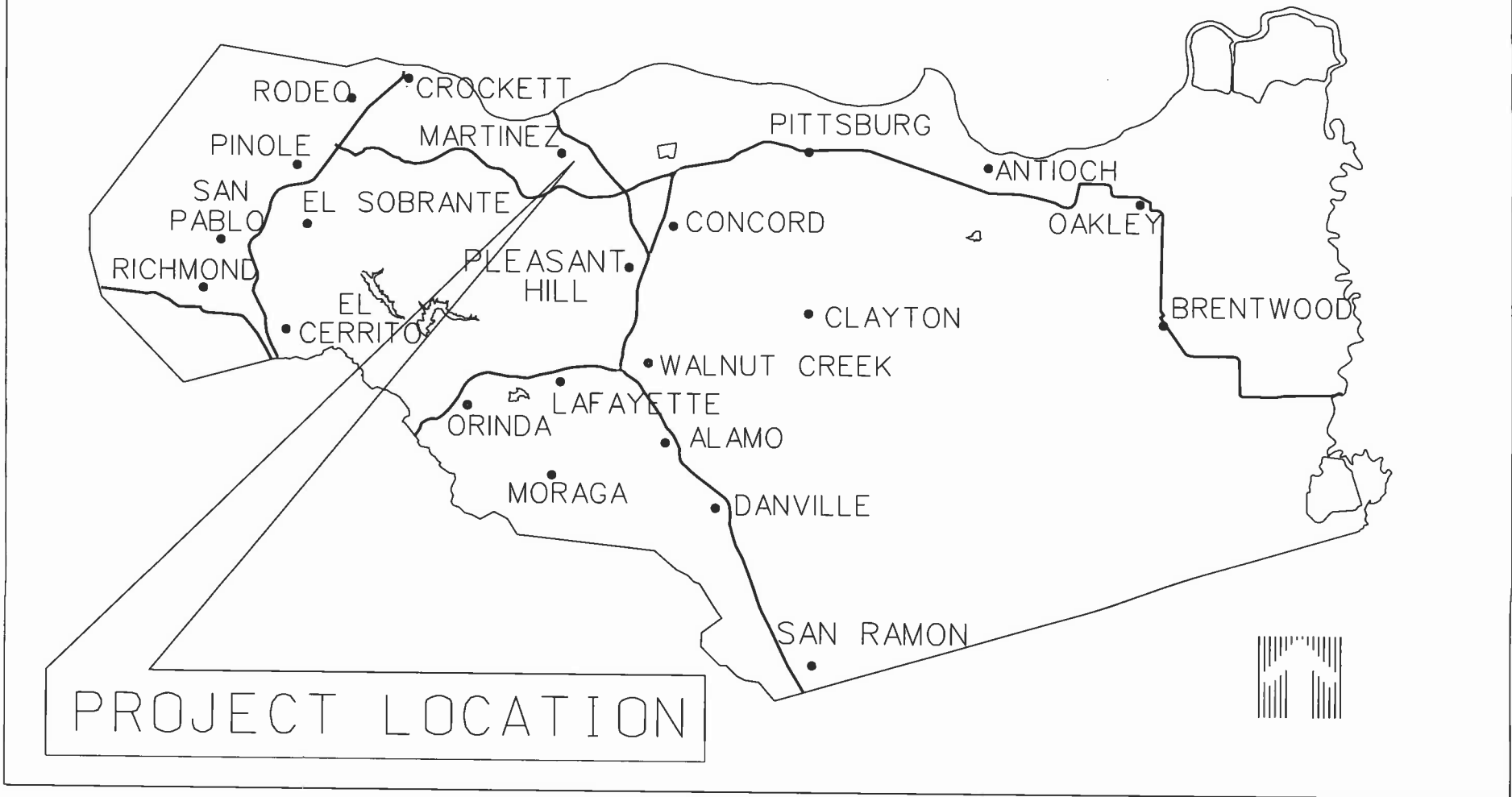
EXHIBIT ONE

Goals of the Measure J CC–TLC Program

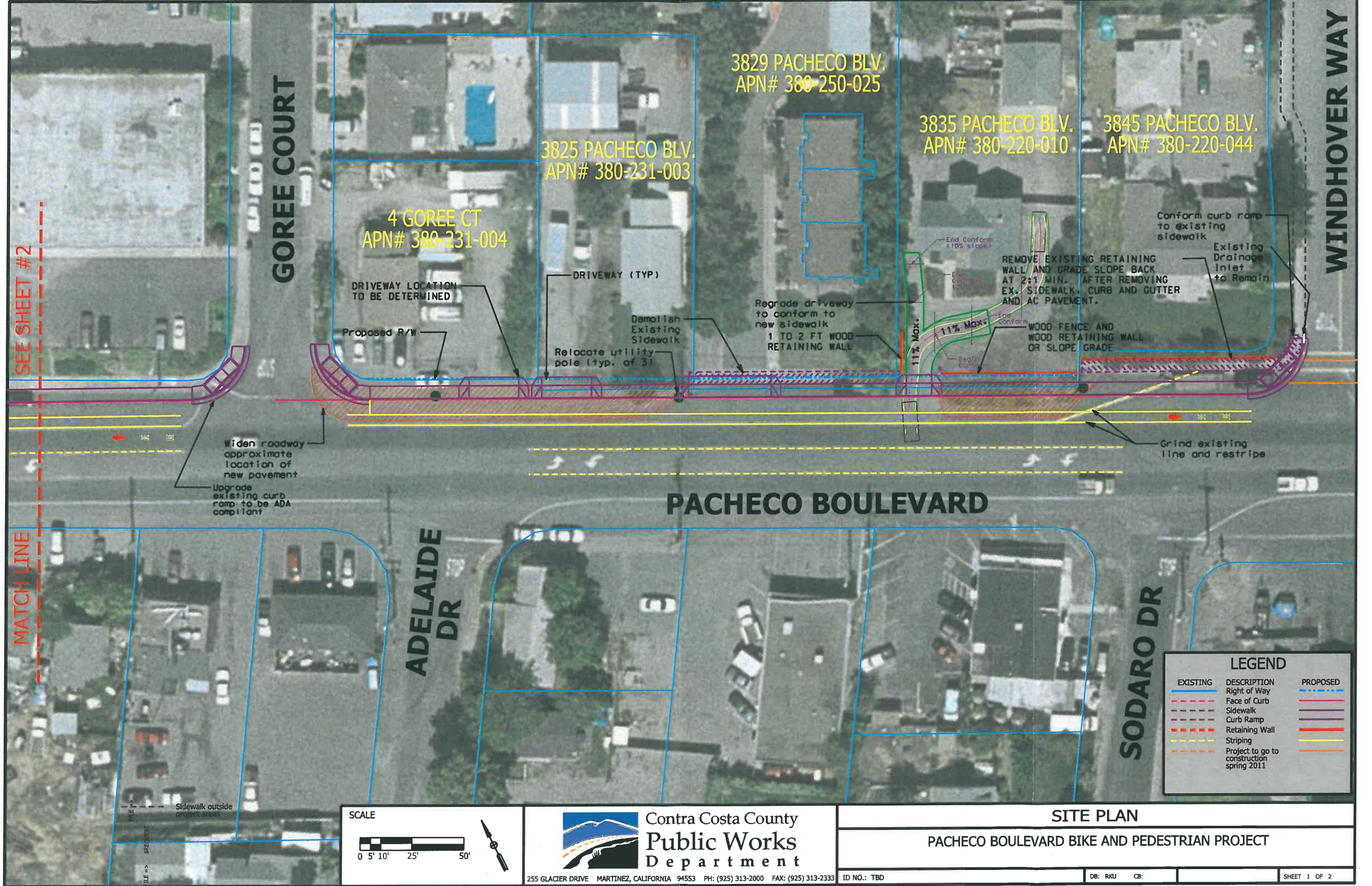
Measure J establishes six goals for the Measure J Transportation for Livable Communities program.

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- Help create walkable, pedestrian-friendly access linking housing and job centers to transit
- Help create affordable housing
- Encourage a mixture of land uses and support a community's development or redevelopment activities
- Provide for a variety of transportation choices to enhance a community's mobility, identity, and quality of life

CONTRA COSTA COUNTY CALIFORNIA



LOCATION MAP: Pacheco Boulevard Sidewalk Project
MARTINEZ, CA



SEE SHEET #2

MATCH LINE

GOREE COURT

WINDHOVER WAY

3829 PACHECO BLV.
APN# 380-250-025

3825 PACHECO BLV.
APN# 380-231-003

3835 PACHECO BLV.
APN# 380-220-010

3845 PACHECO BLV.
APN# 380-220-044

4 GOREE CT
APN# 380-231-004

DRIVEWAY LOCATION
TO BE DETERMINED

DRIVEWAY (TYP)

Proposed R/W

Damolith
Existing
Sidewalk

Relocate utility
pole (typ. of 3)

Regrade driveway
to conform to
new sidewalk
1 TO 2 FT WOOD
RETAINING WALL

11% Max.
11% Max.
11% Max.

REMOVE EXISTING RETAINING
WALL AND GRADE SLOPE BACK
AT 2:1 MIN. AFTER REMOVING
EX. SIDEWALK, CURB AND GUTTER
AND AC PAVEMENT.

WOOD FENCE AND
WOOD RETAINING WALL
OR SLOPE GRADE

Conform curb ramp
to existing
sidewalk

Existing
Drainage
Inlet
to Remain

Widen roadway
approximate
location of
new pavement

Upgrade
existing curb
ramp to be ADA
compliant

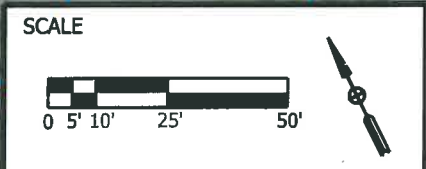
Grind existing
line and restripe

PACHECO BOULEVARD

ADELAIDE
DR

SODARO DR

LEGEND		
EXISTING	DESCRIPTION	PROPOSED
	Right of Way	
	Face of Curb	
	Sidewalk	
	Curb Ramp	
	Retaining Wall	
	Striping	
	Project to go to construction spring 2011	



Contra Costa County
Public Works
Department

255 GLACIER DRIVE MARTINEZ, CALIFORNIA 94553 PH: (925) 313-2000 FAX: (925) 313-2333

SITE PLAN

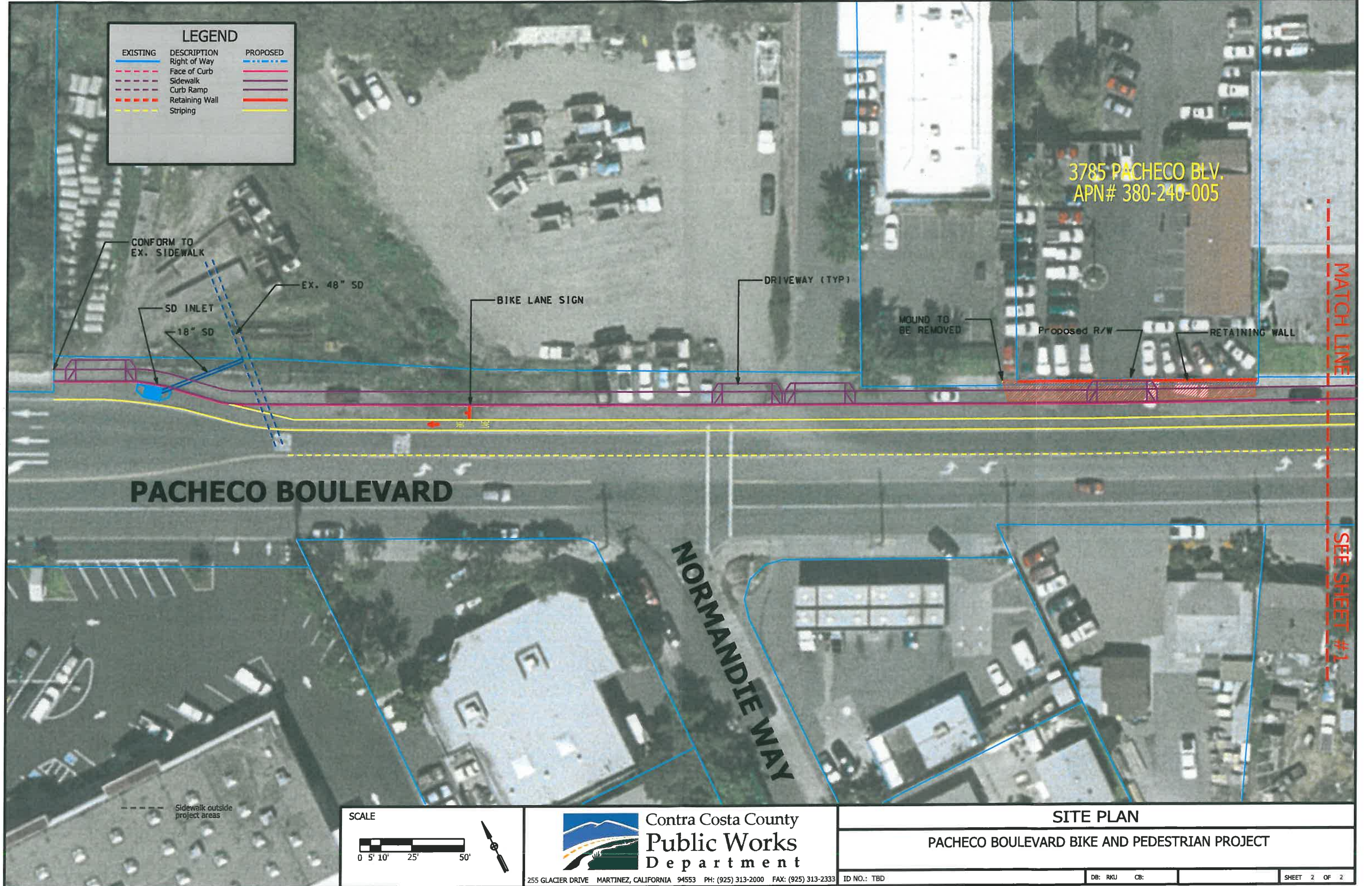
PACHECO BOULEVARD BIKE AND PEDESTRIAN PROJECT

ID NO.: TBD

DB: RJK CB:

SHEET 1 OF 2

LEGEND		
EXISTING	DESCRIPTION	PROPOSED
	Right of Way	
	Face of Curb	
	Sidewalk	
	Curb Ramp	
	Retaining Wall	
	Striping	



3785 PACHECO BLV.
APN# 380-240-005

CONFORM TO
EX. SIDEWALK

SD INLET
18" SD

EX. 48" SD

BIKE LANE SIGN

DRIVEWAY (TYP)

MOUND TO
BE REMOVED

Proposed R/W

RETAINING WALL

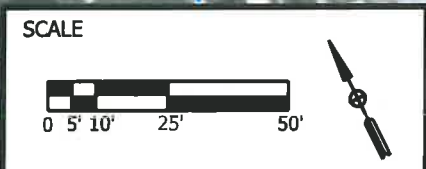
MATCH LINE

PACHECO BOULEVARD

NORMANDIE WAY

SEE SHEET #1

--- Sidewalk outside
project areas



Contra Costa County
Public Works
Department

255 GLACIER DRIVE MARTINEZ, CALIFORNIA 94553 PH: (925) 313-2000 FAX: (925) 313-2333

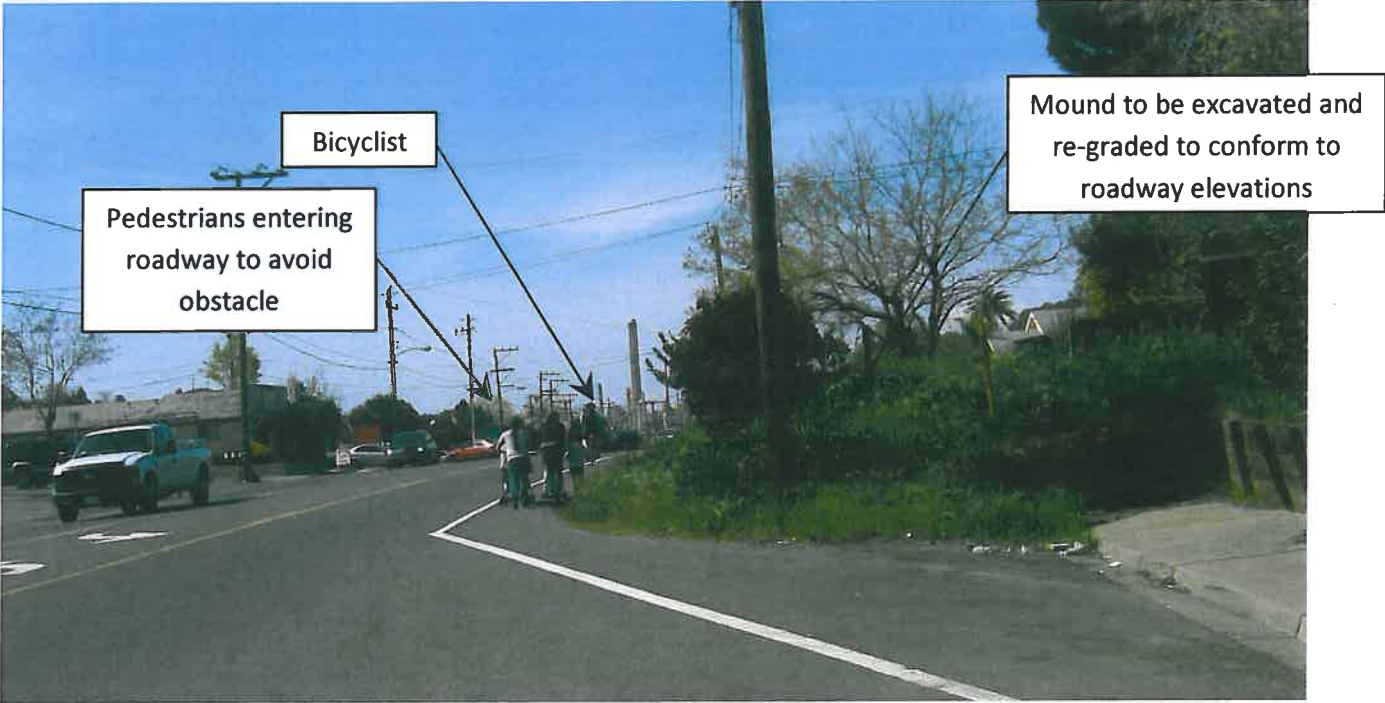
SITE PLAN

PACHECO BOULEVARD BIKE AND PEDESTRIAN PROJECT

ID NO.: TBD	DB: RJK CB:	SHEET 2 OF 2
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Standing at the corner of Windhover Way and Pacheco Boulevard looking west



Standing at the corner of Windhover Way and Pacheco Boulevard looking west



Mound to be excavated and re-graded to conform to roadway/sidewalk elevations



Utility pole to be relocated



Bush to be removed

Install retaining wall behind new sidewalk to catch slope

Students climbing the roadside obstacle to traverse



Utility poles to be relocated

Mound to be excavated and re-graded to conform to roadway/sidewalk elevations

Standing in the middle of the roadside obstacle looking west along Pacheco Boulevard



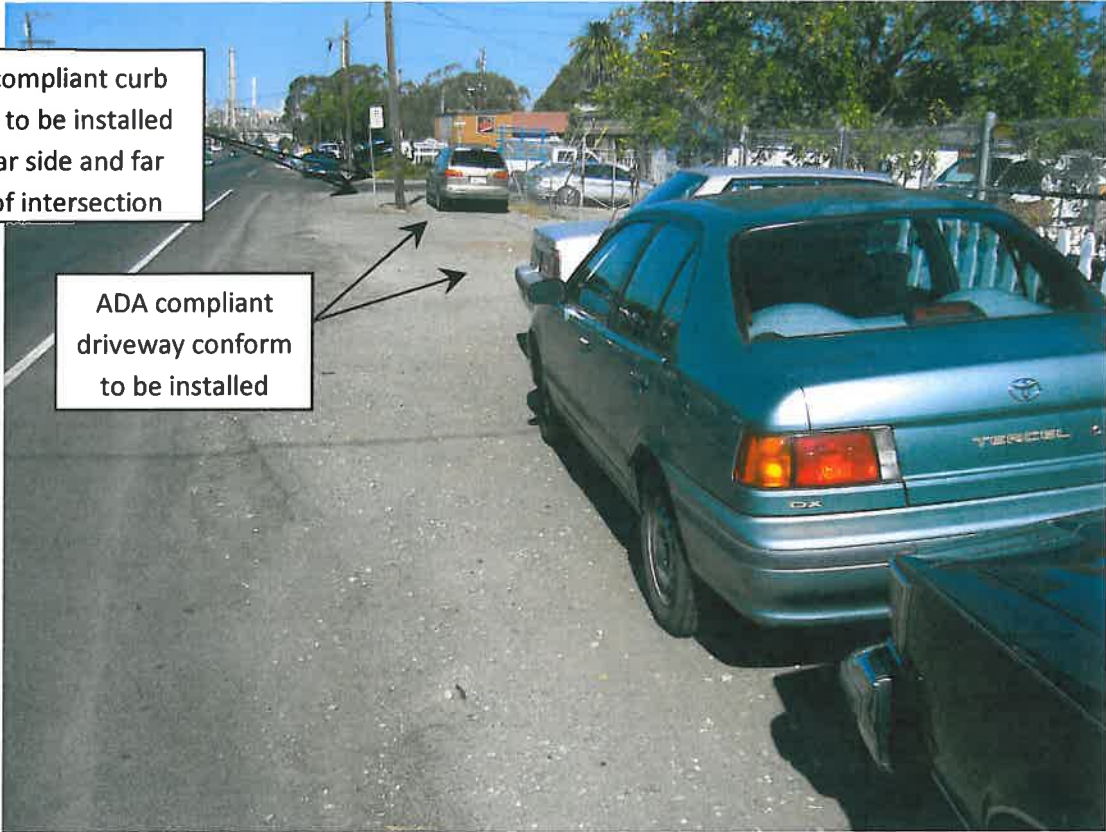
Utility pole to be relocated

ADA compliant driveway conforms to be installed

Standing west of the roadside obstacle looking west along Pacheco Boulevard

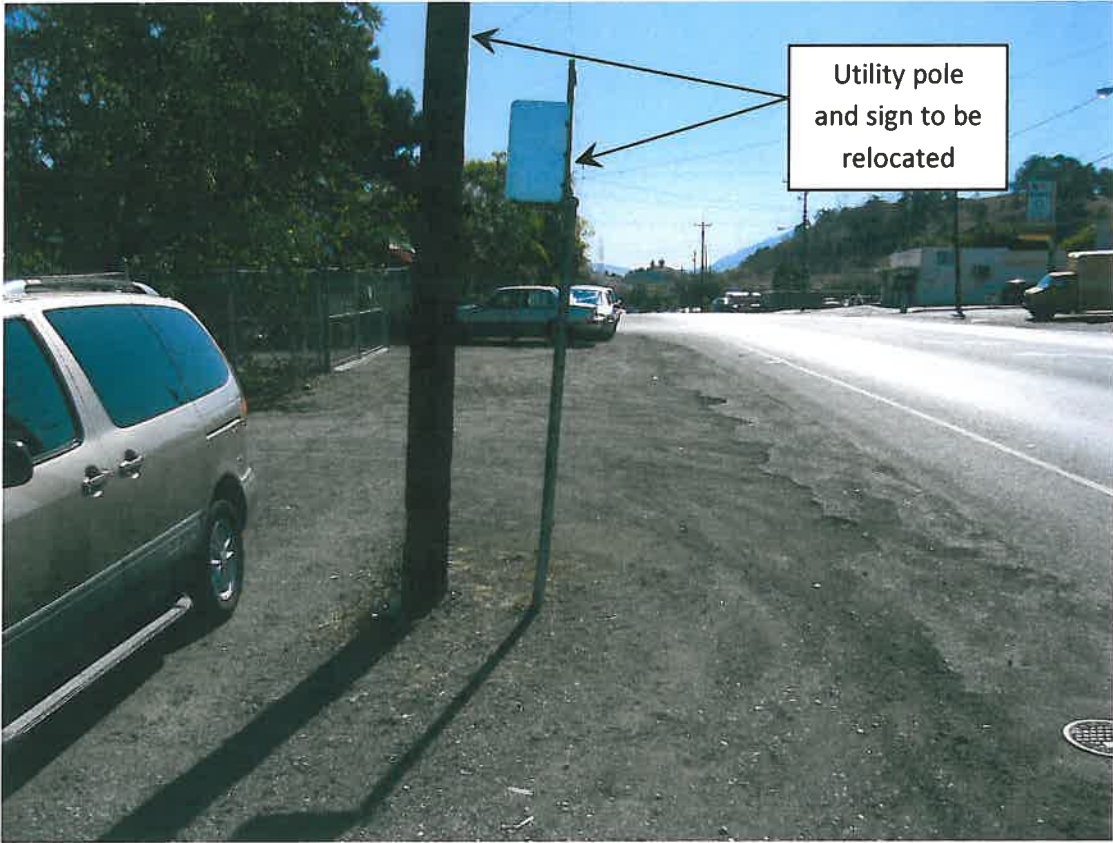
ADA compliant curb ramps to be installed at near side and far side of intersection

ADA compliant driveway conform to be installed



Looking west along Pacheco Boulevard toward the Goree Court intersection

Utility pole and sign to be relocated



Standing at the corner of Goree Court and Pacheco Boulevard looking east



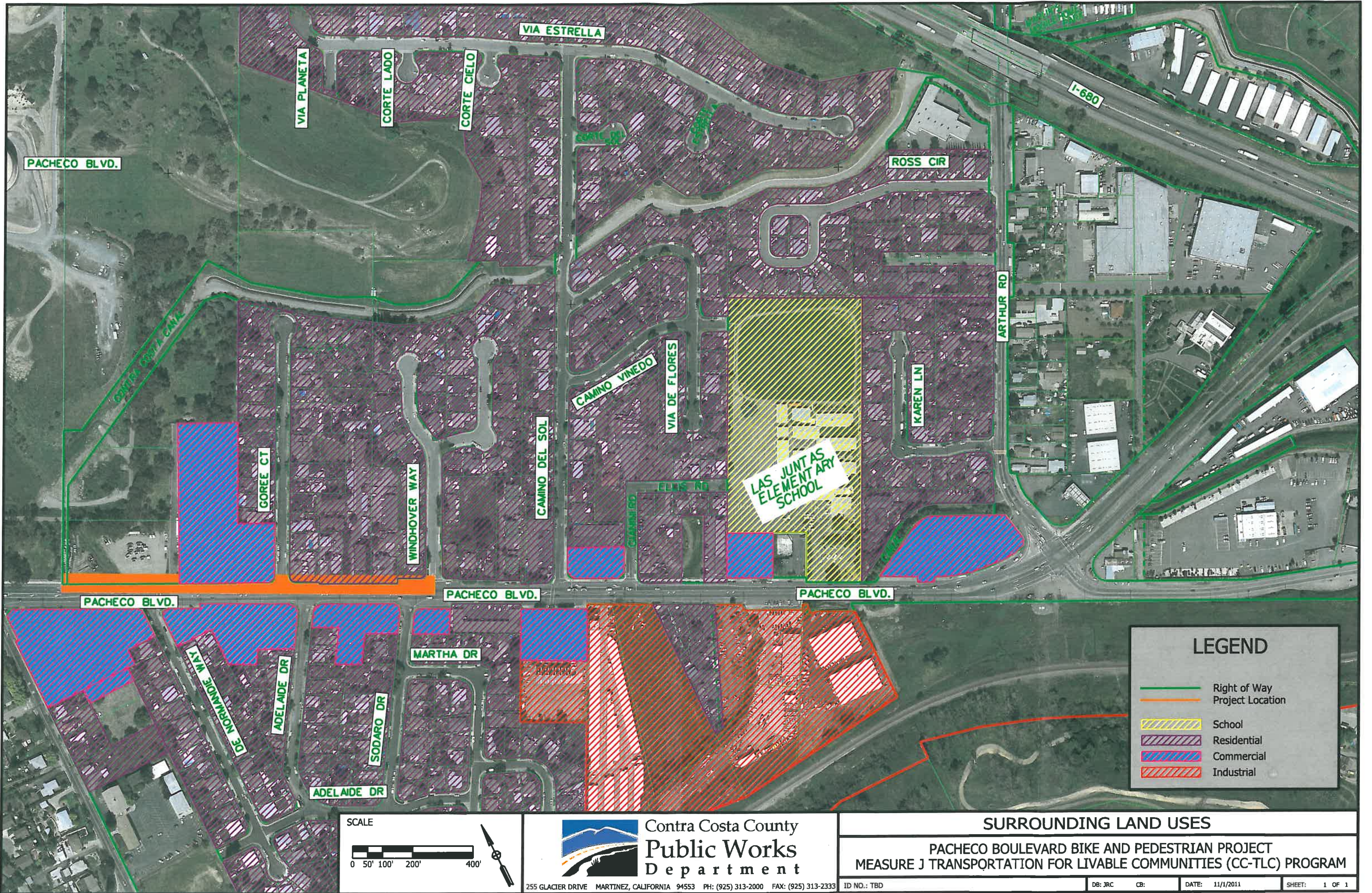
3782 Pacheco Boulevard, AC capped mound to be removed



3782 Pacheco Boulevard, AC capped mound to be removed

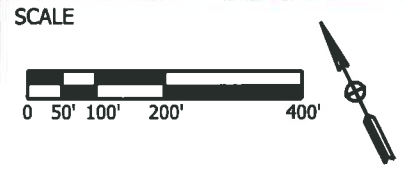


Looking west along Pacheco Boulevard



LEGEND

- Right of Way
- Project Location
- School
- Residential
- Commercial
- Industrial




**Contra Costa County
Public Works
Department**
 255 GLACIER DRIVE MARTINEZ, CALIFORNIA 94553 PH: (925) 313-2000 FAX: (925) 313-2333

SURROUNDING LAND USES

**PACHECO BOULEVARD BIKE AND PEDESTRIAN PROJECT
 MEASURE J TRANSPORTATION FOR LIVABLE COMMUNITIES (CC-TLC) PROGRAM**

ID NO.: TBD	DB: JRC CB:	DATE: 11/1/2011	SHEET: 1 OF 1
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Project Name: Pacheco Boulevard Sidewalk Gap Closure Phase II

Alternative:

Project Location: North side of Pacheco Boulevard between Windhover Way to 230 ft south of Morello Avenue

Assumptions: R = 5.0; TI = 9.0; Design Speed = 45 mph

Project Length (ft): 1,180

Date of Estimate: Nov. 10, 2011

Revision No.	0
Revision Date	
Revised by	

Prepared by:

No.	Description	Quantity	Units	Unit Cost	Total
1	Construction Area Signs	12	EA	\$ 550.00	\$ 6,600
2	Traffic Control System	1	LS	\$ 5,000.00	\$ 5,000
3	Remove Thermoplastic Traffic Stripe	400	LF	\$ 2.00	\$ 800
4	Modify Drainage Inlet	1	EA	\$ 1,500.00	\$ 1,500
5	Clearing and Grubbing	1	LS	\$ 3,000.00	\$ 3,000
6	Saw Cut Pavement Edges	500	LF	\$ 2.00	\$ 1,000
7	Roadway Excavation(includes removal of ex. sw, c&g and pavement)	590	CY	\$ 45.00	\$ 26,550
8	Concrete - Sidewalk	3000	SF	\$ 16.00	\$ 48,000
9	Concrete - Curb & Gutter	500	LF	\$ 40.00	\$ 20,000
10	Minor Concrete - Curb Ramp	4	EA	\$ 3,500.00	\$ 14,000
11	Class 2 Aggregate Base	330	TON	\$ 45.00	\$ 14,850
12	Hot Mix Asphalt (Type A)	145	TON	\$ 190.00	\$ 27,550
13	Roadside Sign - One Post (Bike Lane)	1	EA	\$ 350.00	\$ 350
14	Improvements to 3835 Pacheco Blvd. (grading, paving, drv. ret.wall)	1	LS	\$ 53,000.00	\$ 53,000
15	Wooden fence/2' retaining wall along frontage of 3835 Pacheco Blvd.	75	LF	\$ 100.00	\$ 7,500
16	Improvements to 3825 Pacheco Blvd (Move back 37' long wood fence and hardscaping)	1	LS	\$ 3,000.00	\$ 3,000
17	Improvements to 3845 Pacheco Blvd. Remove wood retaining wall and fill grade with soil and compact	1	LS	\$ 7,000.00	\$ 7,000
18	Thermoplastic Traffic Stripe - Det. 39	440	LF	\$ 3.00	\$ 1,320
19	Thermoplastic Traffic Stripe - Det. 27B	440	LF	\$ 4.00	\$ 1,760
20	Thermoplastic Pavement Marking	50	SF	\$ 20.00	\$ 1,000
21	Mobilization	1	LS	\$ 24,000.00	\$ 24,000
Goree Court to 230 ft south of Morello Avenue					
22	Construction Area Signs	2	EA	\$ 550.00	\$ 1,100
23	Traffic Control System	1	LS	\$ 20,000.00	\$ 20,000
24	Prepare Water Pollution Control Plan	1	LS	\$ 6,000.00	\$ 6,000
25	Remove Thermoplastic Traffic Stripe (700x4/12)	233	SF	\$ 2.00	\$ 466
26	Type A Inlet	1	EA	\$ 4,500.00	\$ 4,500
27	18" SD line	41	LF	\$ 150.00	\$ 6,150
28	Remove Pavement	700	SF	\$ 3.00	\$ 2,100
29	Clearing and Grubbing	1	LS	\$ 20,000.00	\$ 20,000
30	Saw Cut Pavement Edges	700	LF	\$ 2.00	\$ 1,400
31	Roadway Excavation (480 x 12 x 2/27)	430	CY	\$ 45.00	\$ 19,350
32	Curb and Gutter	700	LF	\$ 45.00	\$ 31,500
33	Class 2 Aggregate Base (480 x 12 x 1.3 x 150 /2000)	562	TON	\$ 45.00	\$ 25,290
34	Hot Mix Asphalt (Type A) (480 x 12 x 0.65 x 140/2000)	262	TON	\$ 110.00	\$ 28,820
35	Driveways	3	EA	\$ 5,000.00	\$ 15,000
36	Roadside Sign - One Post (Bike Route)	1	EA	\$ 350.00	\$ 350
37	Masonry Retaining wall (2.5 ft high)	105	LF	\$ 120.00	\$ 12,600
38	Driveway (Regrade parking lot + plus side retaining walls)	1	EA	\$ 50,000.00	\$ 50,000
39	Metal Gateway Modification	1	EA	\$ 20,000.00	\$ 20,000
40	Remove Mound (2.5 x 15 x 120/27)	170	CY	\$ 50.00	\$ 8,500
41	Thermoplastic Traffic Stripe - Det. 21, No Passing Zones	700	LF	\$ 3.00	\$ 2,100
42	Thermoplastic Traffic Stripe - Det. 27B, Right Edge Line	700	LF	\$ 2.00	\$ 1,400

OTHER COSTS BY PHASE:

PLAN	Planning Engineering (TE)	\$ 45,000	CONTRACT ITEMS	\$ 544,000
PE	Preliminary Engineering (Design/Survey)*	\$ 206,000	CONTINGENCY*	\$ 94,000
	Utility Coordination (Design)	\$ 25,000	OTHER COSTS (CON)	\$ 82,000
	Environmental (Environmental, Real Property)	\$ 35,000	SUBTOTAL (CON)	\$ 720,000
R/W	R/W Engineering (Survey)	\$ 20,000	SUBTOTAL (PLAN)	\$ 45,000
	Real Property Labor	\$ 50,000	SUBTOTAL (PE)	\$ 266,000
	R/W Acquisition	\$ 45,000	SUBTOTAL (R/W)	\$ 115,000
CON	Construction Engineering *	\$ 82,000	TOTAL	\$ 1,146,000
	Environmental Monitoring and Mitigation Fees	\$ -	CURRENT YEAR	2011
	SUBTOTAL of OTHER COSTS (ALL)	\$ 508,000	ESCALATION YEAR	2011
			ESCALATION RATE	0.0%

* Preliminary Engineering is 15% of contract items.

* Construction Engineering is 15% of contract items (\$20,000 min.)

* CONTINGENCY is 15% of contract items plus construction engineering. (\$10,000 min.)

TOTAL (in 2011 dollars) \$ 1,146,000

July 7, 2010

Mike Carlson
Assistant Public Works Director
Contra Costa County
Public Works Department
255 Glacier Drive
Martinez, CA 94553-4825

RE: Pacheco Boulevard Sidewalk Project Safe Routes to School Program

Dear Mike,

I am writing in support of the Contra Costa County Public Works Department's application to the Safe Routes to School Program for the Pacheco Boulevard Sidewalk Project. This project will greatly benefit the community in general, but especially the students of Las Juntas Elementary School. The safety of our community's children is of utmost importance and this project will improve their safety by providing necessary sidewalk to help protect students and all other pedestrians from heavy traffic along Pacheco Boulevard. Furthermore, this project will encourage walking and biking, which is something all communities need to do at a time when childhood obesity and air quality issues are of serious concern.

In 2000 I supported a project which constructed sidewalk and bicycle lanes between Las Juntas Elementary School and Camino Del Sol and in 2004 I supported a project which will construct sidewalk and bicycle lanes between Camino del Sol and Windhover Way. This project would serve to continue providing an accessible pedestrian path for the students of Las Juntas Elementary School to the surrounding neighborhoods.

I am pleased to see that the Public Works Department is continuing their efforts to improve safety along this corridor for students by extending the sidewalk and bike lane along the next section of Pacheco Boulevard. The extension of these facilities will allow more students to benefit from existing facilities as well as the new.

Sincerely,



Gayle B. Uilkema
Contra Costa County
District II Supervisor



MARTINEZ UNIFIED SCHOOL DISTRICT

"Together we will ensure diverse paths to success and a quest for knowledge"

SCHOOL BOARD

*Denise Elskan
John Fuller
Vicki Gordon
Kathi McLaughlin
Ron Skrehot*

March 25, 2009



DISTRICT STAFF

Superintendent
Rami Muth

To Whom It May Concern:

Assistant Superintendent

Personnel Services
Rick Rubino

**Chief Business
Official**
Liz Robbins

**Director
Student Services**
Nancy Birenbaum, Ed.D.

I am the Superintendent of Martinez Unified School District and I just became aware of the County's Pacheco Boulevard Sidewalk project. I would like to endorse this special effort. This project would eliminate a barrier to pedestrians where they currently have to walk into the street pavement to get around obstructions. I believe that this project is an important safety improvement. Our students and families currently walk along this route and this would close the remaining sidewalk gap and create a much safer route for our students. We do not provide transportation for our schools so many students at Las Juntas Elementary walk or ride their bikes to school. The Pacheco Boulevard Sidewalk project is an outstanding example of a project that ensures greater safety and one that enhances the community. I wholeheartedly endorse this effort.

SITE

ADMINISTRATORS

Alhambra High
*Sue Mirkovich
Principal*

**Vicente Martinez /
Briones School**
*Carol Adams
Principal*

**Martinez
Junior High**
*Helen Rossi
Principal*

Thank you for your consideration.

**John Mair
Elementary**
*Kathleen Kizziee
Principal*

**John Swett
Elementary**
*Marjorie Pampe
Principal*

Sincerely,

**Rami Muth
Superintendent**

**Las Juntas
Elementary**
*C.J. Cammack
Principal*

**Morello Park
Elementary**
*Ann Dodson
Principal*

**Martinez Adult
Education**
*Kathy Farwell
Director*



MARTINEZ UNIFIED SCHOOL DISTRICT

"Passionately and collaboratively forging paths to excellence"

SCHOOL BOARD

Denise Elsen
Vicki Gordon
Kathi McLaughlin
Mike Menesini
Ron Skrehot

April 2, 2009

DISTRICT STAFF

Superintendent
Dan White, Ed.D.

Assistant
Superintendent
Curriculum and
Instruction
Charles Ferreira, Ed.D.

Jenna Caldwell Civil Engineer
Transportation Engineering Division
Contra Costa County Public Works Department
255 Glacier Drive
Martinez, CA 94553-4825

Assistant
Superintendent
Administrative
Services
Timothy Rahill
Director
Student Services
Nancy Birenbaum, Ed.D.

Director
Personnel Services
Rick Rubino

Dear Jenna:

SITE ADMINISTRATORS

Alhambra High
Toni Taylor,
Principal

I was recently made aware of the County's Pacheco Boulevard Sidewalk project and wish to endorse their effort to improve this area for pedestrians. Many of our children walk to school along this route and the safety benefits that will result from this project are so very important to those children.

Vicente Martinez /
Briones School
Matt Smith,
Principal

Thank you for your consideration on this important issue.

Martinez
Junior High
Helen Rossi,
Principal

Sincerely,

Vicki Gordon
MUSD School Board Member

John Muir
Elementary
Kathleen Kizziee,
Principal

John Swett
Elementary
Mary Delaney,
Principal

Las Juntas
Elementary
Sue Mirkovich,
Principal

Morello Park
Elementary
Ann Dodson
Principal

Martinez Adult
Education
Marshall Burgamy,
Director





LAS JUNTAS ELEMENTARY SCHOOL

MARTINEZ UNIFIED SCHOOL DISTRICT

SCHOOL BOARD

Denise Elsen
John Fuller
Vicki Gordon
Kathi McLaughlin
Ron Skrehot

March 31, 2009

DISTRICT STAFF

Superintendent
Rami Muih

Assistant Superintendent
Personnel Services
Rick Rubino

Chief Business
Official
Liz Robbins

Director
Student Services
Nancy Birenbaum, Ed.D.

SITE ADMINISTRATORS

Alhambra High
Sue Mirkovich
Principal

Vicente Martinez /
Briones School
Carol Adams
Principal

Martinez
Junior High
Helen Rossi
Principal

John Muir
Elementary
Kathleen Kizziee
Principal

John Swett
Elementary
Marj Pampe
Principal

Las Juntas
Elementary
C.J. Cammack
Principal

Morello Park
Elementary
Ann Dodson
Principal

Martinez Adult
Education
Kathy Farwell
Director

Dear Sir/Madam,

This letter is to express my complete support for the County's Pacheco Boulevard Sidewalk project. I wish to endorse this project completely. Providing a safe route of transportation is not only crucial to student safety, it allows members of a community to feel their school is a respected place, easily accessible by all. A large number of our students and families walk to school and would greatly benefit from the completion of this project.

As the school principal I work with all members of our school community to ensure a cooperative effort in educating students and improving the community. This project will help our community members feel more connected to their school. Many research studies document the more connected a student and their family feels to a school, the greater the academic achievement.

Thank you for your time and consideration regarding this proposal. I look forward to sharing the exciting news of a safer route to school with our students and community members. Please contact me should you have any further questions for me regarding this project. Have a wonderful day.

Sincerely,

CJ Cammack, Principal
Las Juntas Elementary School
Martinez Unified School District
ccammack@martinez.k12.ca.us
925-335-5830 ext. 3380



Application Form

Measure J Transportation for Livable Communities (CC-TLC) Program Funds

1. PROJECT INFORMATION

Project Name..... Golf Club Road Bridge Widening/Replacement Project

Project Location Golf Club Road (between Old Quarry Road and Contra Costa Boulevard)

Sponsor..... City of Pleasant Hill

Implementing agency City of Pleasant Hill

Partner agencies N/A

Contact for project..... Mario Moreno, City Engineer

(925) 671-5252, mmoreno@ci.pleasant-hill.ca.us

100 Gregory Lane, Pleasant Hill, CA 94523

2. FUNDING

In thousands of dollars, please provide the estimated cost of the proposed project, the amount of CC-TLC funds requested, and any other funds that have been committed to the project.

<i>Source</i>	<i>Amount</i>
Total project cost †	\$3,356,000
Requested CC-TLC funds	\$521,000
Federal Highway Bridge Program (HBR)	\$2,835,000
Unfunded balance	\$6,712,000

† Minimum request is \$50,000

‡ Add other committed funding sources as needed

3. POTENTIAL PHASING

The proposed project will replace the existing roadway bridge along Golf Club Road with a wider bridge structure and approach that will accommodate Class II and Class III bike facilities and 5-foot sidewalks on both sides, between Old Quarry Road and Contra Costa Boulevard. The City has secured \$2,835,000 in Federal Highway Bridge Program (HBR) funding for the project, and is seeking \$521,000 in Measure J CC-TLC funds to fully fund the project construction. Project components cannot be removed to decrease project cost, since all structural, pedestrian, and bicycle components are designed to meet Federal funding requirements and must be implemented as part of the project. Should Measure J funding not be awarded to the project, the City will seek alternative funding opportunities, resulting in a delay in project construction.

4. PROJECT DESCRIPTION

The purpose of the Golf Club Road Bridge Replacement/Widening project is to replace the existing structurally deficient bridge structure along Golf Club Road (over Grayson Creek) and construct a new bridge structure and roadway approaches to accommodate Class II bike lanes and 5-foot sidewalks along both sides of the roadway between Old Quarry Road and College Drive (see attached new roadway/bridge cross-section). Class III bike routes will be implemented along sections of the roadway between College Drive and Contra Costa Boulevard (outside of the approach area to the new bridge). The project will also relocate an existing 24-inch water main pipe along Golf Club Road to accommodate the new bridge structure, which currently serves the DVC Transit Center, the DVC campus, and the City's DVC Plaza Priority Development Area (PDA). The existing road bridge was constructed in 1953, and it is not pedestrian or bicycle friendly.

Golf Club Road is the primary arterial roadway in the area, connecting the DVC Transit Center, DVC campus, and DVC Plaza PDA to the nearby Sun Valley Shopping Mall, Contra Costa Boulevard (the principle Regional Route of Significance in the City), and I-680 Freeway (see attached project location map). The City is currently in a multi-year effort to install Class II bike lanes and sidewalks along Contra Costa Boulevard, and the proposed bicycle and pedestrian improvements along Golf Club Road will provide vital connections from the DVC Campus to those facilities.

The proposed project is located within the City's DVC Plaza PDA, and the proposed improvements will greatly support the City's long term vision of improving pedestrian, bicycle, and transit access to and around the PDA. The project fully supports the intent and goals of the Measure J CC-TLC program.

5. PROPOSED SCHEDULE

In the following table, enter the expected beginning and ending *month and year* for each of the phases listed. If the phase has been finished, enter "Completed" in the "To" column.

<i>Phase</i>	<i>From</i>	<i>To</i>
Preliminary Design & Planning		Complete
Design	February 2009	May 2012
Environmental Clearance	July 2011	December 2011
Right-of-Way	February 2009	March 2012
Construction	May 2012	December 2015

6. ABILITY TO MEET CRITERIA

Describe how the proposed project would meet the following four criteria:

Achievement of CC-TLC Goals:

Identify which of the six goals of the CC-TLC program the proposed project would help achieve — a project need not meet all of them — and *briefly* describe how it would do so (see Exhibit One for these six goals)

The proposed project greatly supports all six CC-TLC goals. The project is located within the City's DVC Plaza Priority Development Area (PDA), and it provides vital pedestrian and bicycle connection to along Golf Club Road between the PDA and the nearby DVC Transit Center and Sun Valley Shopping Center. By widening Golf Club Road (within the project limit) to accommodate new bicycle facilities and wider sidewalks, the project helps promote pedestrian and bicycle friendly neighborhoods and business districts along Golf Club Road and the PDA. The DVC Transit Center and multi-family residential neighborhoods are adjacent to the west end of the project limit, and the Sun Valley Shopping Center lies just to the east of the project limit. The City currently has plans to redevelop the DVC Plaza PDA with mix use retail/residential development, and this project will provide the public infrastructure to support that effort. Lastly, given the close proximity to the DVC Transit Center, which provides vital bus connections to the Pleasant Hill/Concord/Walnut Creek BART stations, the project will greatly encourage the residents/college students to use bicycling, walking, and transit as alternative modes of transportation in and around the PDA.

Feasibility: Describe why the project is feasible and any outstanding issues in the project development process — design, environmental clearance, right-of-way purchase, and PS&E — or funding of the project

The project does require right of way acquisition and engineering, and both have been substantially completed. To date, the City has secured over 15 parcels in forms of property acquisition, permanent utility easements, and construction easements. Only one private parcel (Briarwood Apartments) has required the City to move forward with a Resolution of Necessity and eminent domain proceedings, and the process is anticipated to be finalized by March 2012.

The City has already obtained NEPA clearance for

the project (Categorical Exclusion), and is working with Caltrans on a NEPA Revalidation for the three year construction phasing plan. The City is currently also working to obtain the necessary permits from U.S. Army Corps of Engineers, Regional Water Quality Control Board, Contra Costa Flood Control District, and the Department of Fish and Game, all of which is anticipated to be obtained by March 2012.

Local and policy support: Identify local policies that support the project and the integration of the project with other local efforts, as well as other support from the general public, the RTPCs and other relevant agencies

Given the wide range of benefit to motorists, transit services, pedestrians, and cyclists in the area, the project is greatly supported by DVC, transit operators, business owners, residents, and City Council. The project is vital to the City's on-going effort to promote walking and biking in the City, as well as the City's effort to redevelop the DVC Plaza PDA and encourage alternative modes of transportation among the DVC faculty and student body.

Matching funds: Identify any funds from other sources that are or would be committed to the project

The City has secured \$2,835,000 in Federal Highway Bridge Program (HBR) Funds for the replacement of the existing Golf Club Road bridge and the relocation of the 24-inch water main adjacent to the bridge structure.

The requested \$521,000 in Measure J CC-TLC funding will fully fund the project implementation.

EXHIBIT ONE

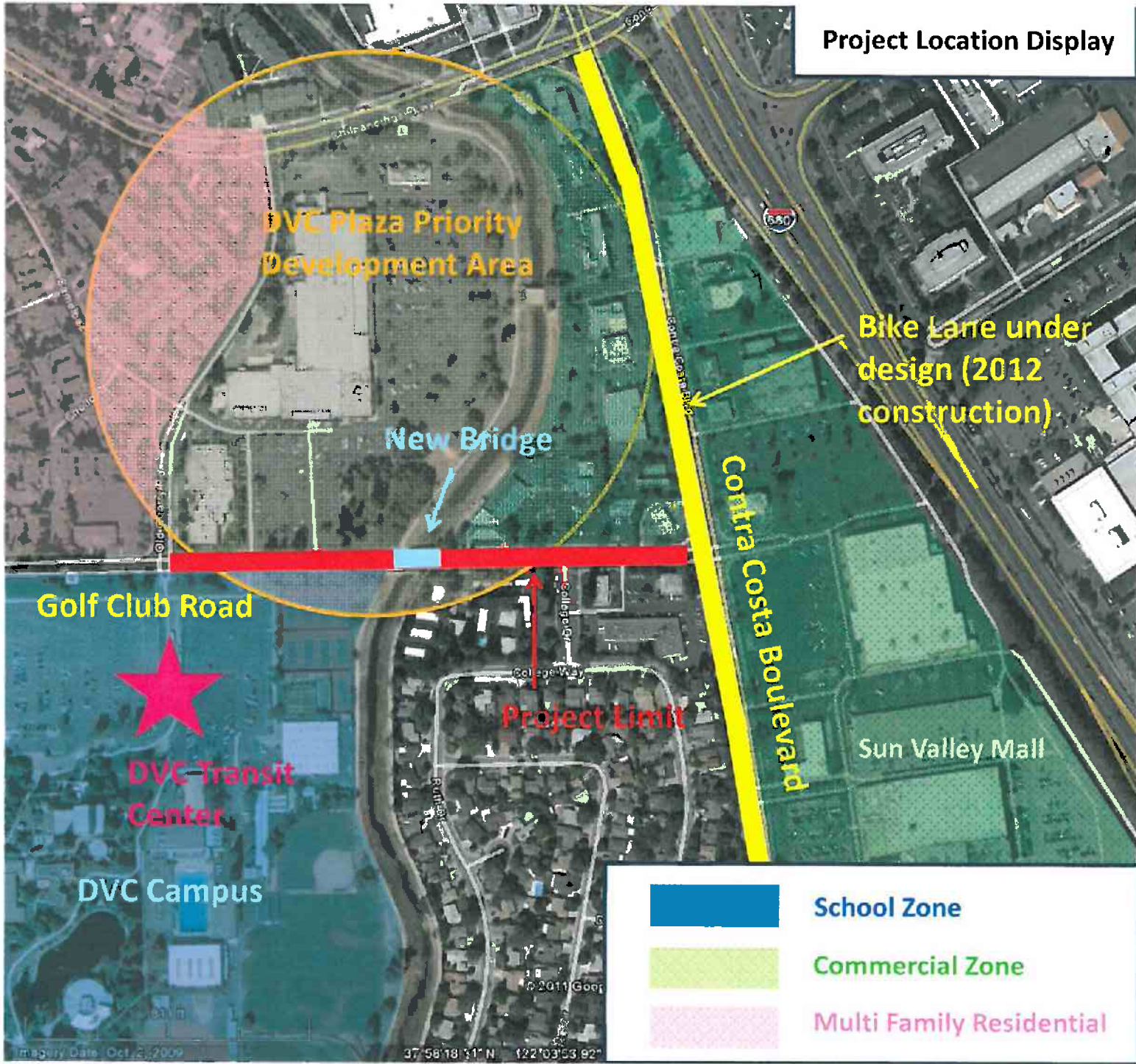
Goals of the Measure J CC-TLC Program

Measure J establishes six goals for the Measure J Transportation for Livable Communities program.

- Help create walkable, pedestrian-friendly neighborhoods and business districts
- Promote innovative solutions, including compact building design and context-sensitive site planning that is integrated with the transportation system
- Help create walkable, pedestrian-friendly access linking housing and job centers to transit
- Help create affordable housing
- Encourage a mixture of land uses and support a community's development or redevelopment activities
- Provide for a variety of transportation choices to enhance a community's mobility, identity, and quality of life

Application Attachments

Project Location Display



DVC Plaza Priority Development Area

New Bridge

Bike Lane under design (2012 construction)

Golf Club Road

DVC Transit Center

DVC Campus

Project Limit

Contra Costa Boulevard

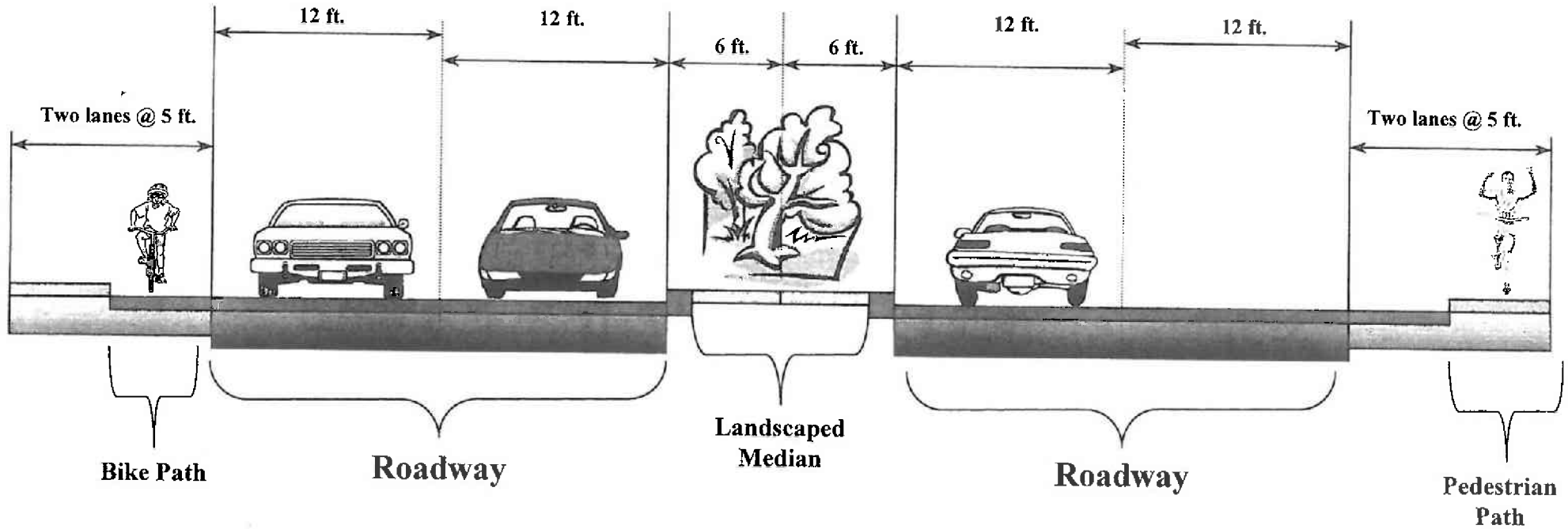
Sun Valley Mall

- School Zone
- Commercial Zone
- Multi Family Residential

Imagery Date: Oct 2, 2009

37° 58' 18.11" N 122° 03' 53.92" W

Typical Cross-Section

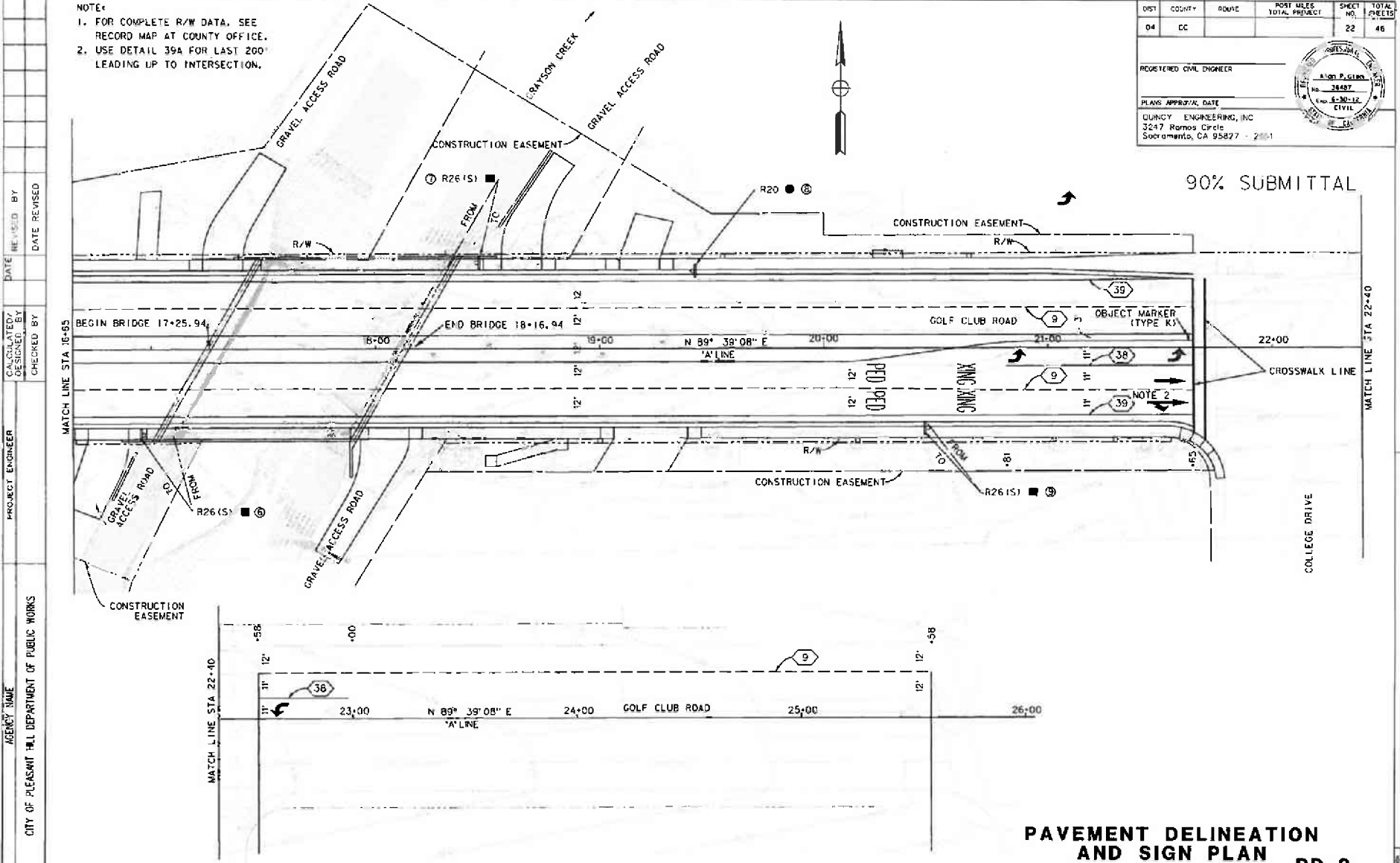


- NOTE:
 1. FOR COMPLETE R/W DATA, SEE RECORD MAP AT COUNTY OFFICE.
 2. USE DETAIL 39A FOR LAST 200' LEADING UP TO INTERSECTION.

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET NO.	TOTAL SHEETS
04	CC			22	46

REGISTERED CIVIL ENGINEER	
PLANS APPROVAL DATE	
QUINCY ENGINEERING, INC. 3247 Ramos Circle Sacramento, CA 95827 - 2001	

90% SUBMITTAL



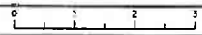
PROJECT ENGINEER: _____
 AGENCY NAME: CITY OF PLEASANT HILL DEPARTMENT OF PUBLIC WORKS
 DATE REVISION BY: _____
 DATE REVISION BY: _____
 CALCULATED/DESIGNED BY: _____
 CHECKED BY: _____

PAVEMENT DELINEATION AND SIGN PLAN PD-2

THIS PLAN ACCURATE FOR PAVEMENT DELINEATION AND SIGNING ONLY

SCALE 1" = 20'

FOR REDUCED PLANS ORIGINAL SCALE IS IN INCHES



PLAN FILE -> drawings -> city -> pleashill -> 04-00-00-000000000000

CU 00000

EA 000000

DATE PLOTTED -> 11/14/2011
 TIME PLOTTED -> 11:45:33 AM

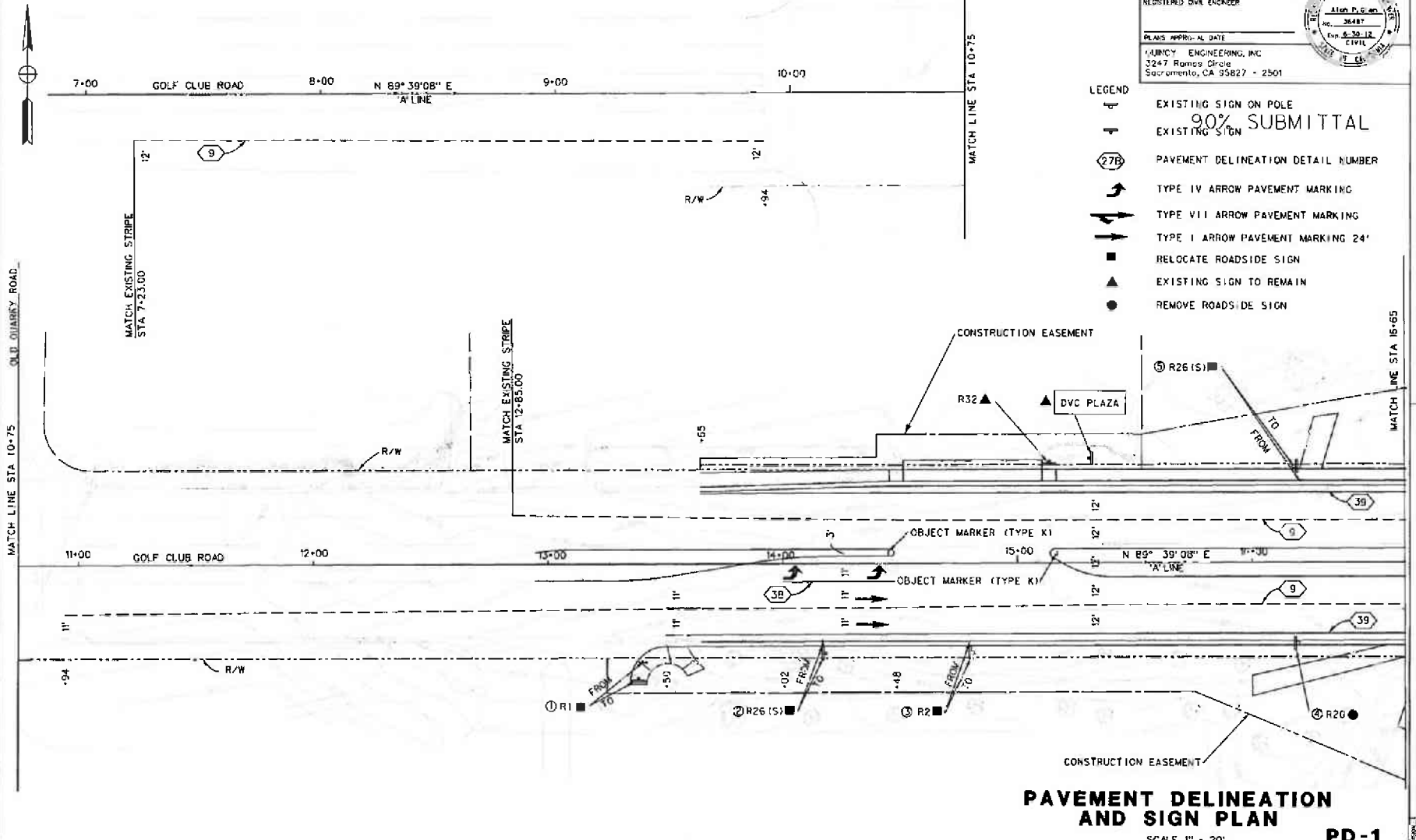
NOTE: FOR COMPLETE R/W DATA, SEE RECORD MAP AT COUNTY OFFICE.

DIST	COUNTY	ROUTE	DIST. MILES TOTAL PROJECT	SHEET NO.	TOTAL SHEETS
04	CC			21	46

REGISTERED CIVIL ENGINEER
 PLAN APPROVAL DATE
 AGENCY ENGINEERING, INC.
 3247 Ramos Circle
 Sacramento, CA 95827 - 2501



- LEGEND**
- EXISTING SIGN ON POLE
 - EXISTING SIGN
 - PAVEMENT DELINEATION DETAIL NUMBER
 - TYPE IV ARROW PAVEMENT MARKING
 - TYPE VII ARROW PAVEMENT MARKING
 - TYPE I ARROW PAVEMENT MARKING 24'
 - RELOCATE ROADSIDE SIGN
 - EXISTING SIGN TO REMAIN
 - REMOVE ROADSIDE SIGN



AGENCY NAME	PROJECT ENGINEER	DATE PREPARED BY	DATE REVISION BY
CITY OF PLEASANT HILL DEPARTMENT OF PUBLIC WORKS			

MATCH LINE STA 10+75 OLD QUARRY ROAD

MATCH LINE STA 10+75

MATCH EXISTING STRIPE STA 7+23.00

MATCH EXISTING STRIPE STA 12+85.00

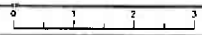
MATCH LINE STA 10+75

MATCH LINE STA 15+65

PAVEMENT DELINEATION AND SIGN PLAN
 SCALE 1" = 20'
PD-1

THIS PLAN ACCURATE FOR PAVEMENT DELINEATION AND SIGNING ONLY

FOR REDUCED PLANS ORIGINAL SCALE IS BY INCHES



CU 00000 EA 000000

DATE PRINTED - 11/4/01
 FILE NO. - 001001
 PROJECT NO. - 001001



Application Form

Measure J Transportation for Livable Communities (CC-TLC) Program Funds

1. PROJECT INFORMATION

Project Name..... Citywide Trail Crossing Enhancement Project

Project Location Six various trail crossing locations along the Contra Costa Canal Trail (Chilpancingo Parkway, Viking Drive, Gregory Lane, Boyd Road, Oak Park Boulevard, and Putnam Boulevard) and two trail crossing locations along the EBMUD Trail (Oak Park Boulevard and Astrid Drive).

Sponsor/Implementing Agency..... City of Pleasant Hill

Partner agencies None

Contact for project..... Eric Hu, Associate Civil Engineer
 Phone: (925) 671-5203, Email: ehu@ci.pleasant-hill.ca.us
 Address: 100 Gregory Lane, Pleasant Hill, CA 94523

2. FUNDING

In thousands of dollars, please provide the estimated cost of the proposed project, the amount of CC-TLC funds requested, and any other funds that have been committed to the project.

<i>Source</i>	<i>Amount</i>
Total project cost †	\$1,286,000
Requested CC-TLC funds	\$371,200
Prop. 1B Transportation Bond	\$492,000
TDA-Article 3 Grant	\$95,000
State Safe Route to School Grant	\$327,800
Unfunded balance	\$2,572,000

† Minimum request is \$50,000

3. POTENTIAL PHASING

The project scope consists of constructing access and visibility enhancements at eight key trail crossings along the Contra Costa Canal Trail and the EBMUD Trail networks. The total project construction cost is estimated at \$1,286,000, of which the City has secured \$914,800. If Measure J funding is not awarded to fully fund the project, the City will be required to remove multiple trail crossing locations from the project scope until the current funding level is adequate to construct the various crossing improvements.

4. PROJECT DESCRIPTION

The purpose of the project is to construct/install access and visibility enhancements at eight key trail crossings along the Contra Costa Canal and EBMUD Regional Trail networks in the City of Pleasant Hill. Trail crossing locations currently identified for enhancement along the Contra Costa Canal Trail are at Chilpancingo Parkway, Viking Drive, Gregory Lane, Boyd Road, Oak Park Boulevard, and Putnam Boulevard. Trail crossings along the EBMUD Trail that will be enhanced are at Astrid Drive and Oak Park Boulevard.

Enhancement features proposed for each trail crossing location include raised colored crosswalk, advanced yield “sharktooth” striping, in-pavement flashing system, LED signs, advanced LED signage, radar speed feedback signs, ADA curb ramps, and connector sidewalks from the various trail crossings to the nearby existing sidewalk networks. At the Gregory Lane/Contra Costa Canal Trail crossing location, a new pedestrian signal will be installed given the speed and traffic volume along Gregory Lane.

The project is currently at 95% design, and is scheduled for Spring 2012 construction. The City is working jointly with the East Bay Regional Parks District, Contra Costa Water District, United States Bureau of Reclamation, and the City of Walnut Creek on the project implementation. Once the project is constructed, the City will maintain all electrical hardware, concrete ramp/sidewalk, and roadway improvements at the various crossing locations. East Bay Regional Parks District will continue to maintain the trail pavement leading up to the new concrete sidewalk and curb ramps. See attachments for crossing locations, sample photos of enhancement features, and photos of the various crossings to be enhanced.

5. PROPOSED SCHEDULE

In the following table, enter the expected beginning and ending *month and year* for each of the phases listed. If the phase has been finished, enter “Completed” in the “To” column.

<i>Phase</i>	<i>From</i>	<i>To</i>
Preliminary Design & Planning		Completed
Design	April 2011	December 2011
Environmental Clearance		Completed
Right-of-Way	June 2011	December 2011
Construction	April 2012	August 2012

6. BILITY TO MEET CRITERIA

Describe how the proposed project would meet the following four criteria:

Achievement of CC-TLC Goals:

Identify which of the six goals of the CC-TLC program the proposed project would help achieve — a project need not meet all of them — and *briefly* describe how it would do so (see Exhibit One for these six goals)

The proposed project will greatly help create walkable, pedestrian-friendly, and bicycle friendly neighborhoods in the City of Pleasant Hill, as well as greatly enhancing walkable, pedestrian friendly access between numerous residential/commercial districts in the City to the various transit centers (e.g. DVC Transit Center and Pleasant Hill BART Station). The Contra Costa Canal Trail provides vital north-south connection throughout the City of Pleasant Hill, passing by DVC campus, six public school campuses, the DVC Plaza Priority Development Area (PDA), and a number of shopping centers. The Canal trail is also one of two pedestrian and bicycle friendly east-west routes in the City that accesses the Pleasant Hill BART station across the I-680 corridor. The EBMUD trail is a City maintained Class I trail that spans between the Canal Trail and the City's Downtown District (which is also a PDA). By enhancing vital arterial crossings along these two trail networks, the project will greatly encourage residents in Pleasant Hill, Walnut Creek, and Martinez to use walking and biking as viable alternative modes of transportation to access various regional destinations.

Feasibility: Describe why the project is feasible and any outstanding issues in the project development process — design, environmental clearance, right-of-way purchase, and PS&E — or funding of the project

The proposed project is currently at 95% detailed design, and the City has already obtained CEQA and NEPA approval for the project. City staff has already obtained permission from the United States Bureau of Reclamation to work within the canal right of way, and is currently working with the Contra Costa Water District on obtaining construction permits for the improvements. The City is also working with City of Walnut Creek staff on the design of the canal trail crossing enhancements at Putnam Boulevard (half of the road is in the City of Walnut Creek). The requested Measure J funding amount would fully

fund the construction of the proposed project improvements.

Local and policy support: Identify local policies that support the project and the integration of the project with other local efforts, as well as other support from the general public, the RTPCs and other relevant agencies

Given the frequent trail usage by residents in the City, the project is strongly supported by city residents and City Council. The project strongly supports the City's on-going effort to promote alternative modes of transportation in the City, as specified in the City's General Plan.

Matching funds: Identify any funds from other sources that are or would be committed to the project

The City has secured \$492,000 in Proposition 1B Transportation Bond Funds, \$327,800 in State Safe Routes to School grant funds, and \$95,000 in State Transportation Development Act – Article 3 funds for the design and construction of the project.

EXHIBIT ONE

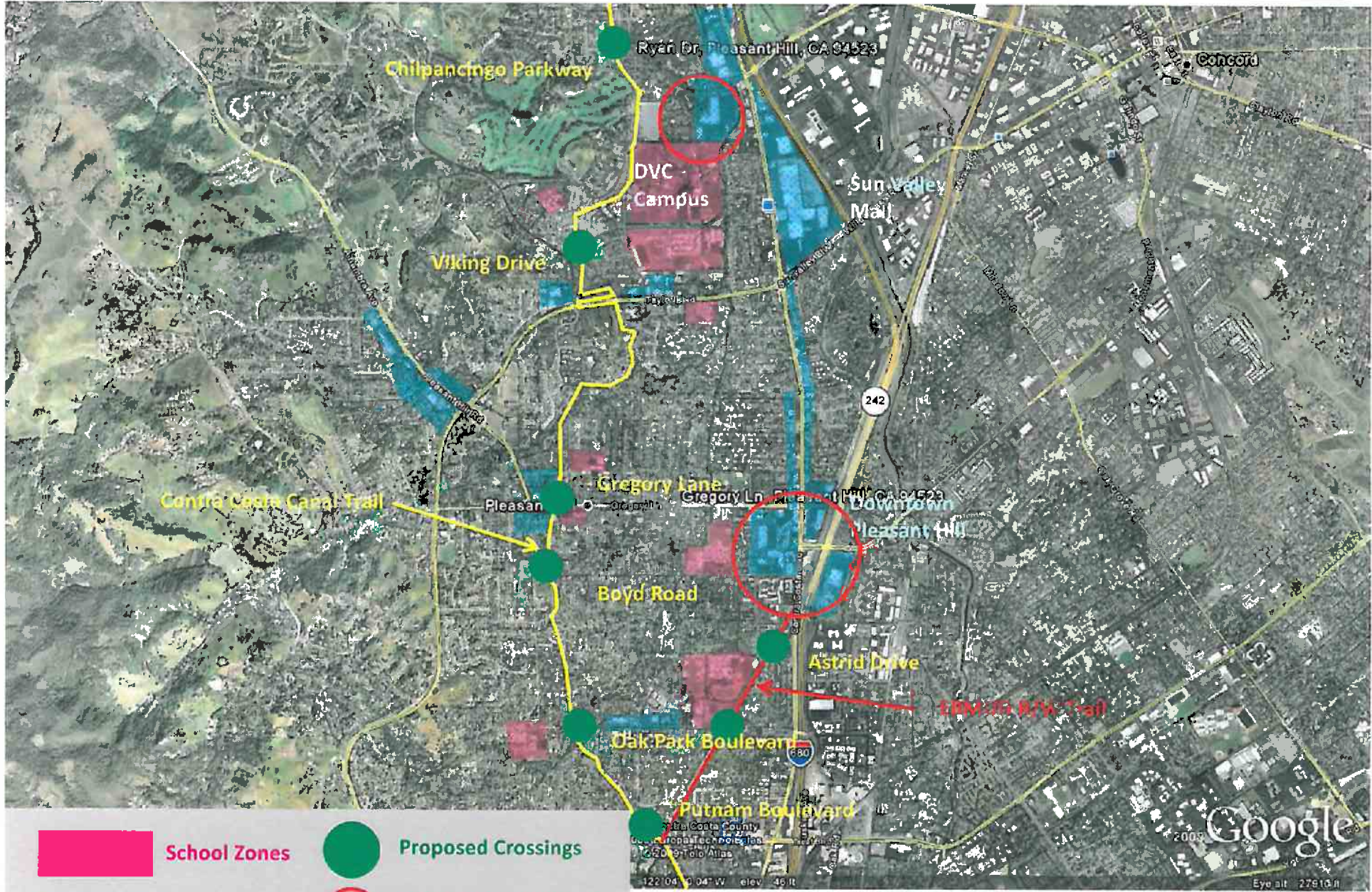
Goals of the Measure J CC-TLC Program

Measure J establishes six goals for the Measure J Transportation for Livable Communities program.

- Help create walkable, pedestrian-friendly neighborhoods and business districts
- Promote innovative solutions, including compact building design and context-sensitive site planning that is integrated with the transportation system
- Help create walkable, pedestrian-friendly access linking housing and job centers to transit
- Help create affordable housing
- Encourage a mixture of land uses and support a community's development or redevelopment activities
- Provide for a variety of transportation choices to enhance a community's mobility, identity, and quality of life

Trail Crossing Locations

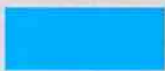
Crossing Location Map



School Zones



Proposed Crossings



Commercial Zones



Priority Development Area (PDA)

Improvement Sample Photos

Sample In-Pavement Flasher System with LED Signs



*Smart Crosswalk™ Installation — Cisco System Campus, Petaluma California
Automatic Bollard Activation, LED Enhanced Pedestrian Crossing ActiveSign™*

Sample Raised Color Crosswalk Treatment



Improvement Layouts

EQUIPMENT SCHEDULE			
No.	Type	#	SPECIAL REQUIREMENTS
(A)	1-B POLE	B	INSTALL CONTROLLER CABINET ON POLE PER MANUFACTURER'S SPECIFICATIONS. INSTALL W11-2 LED PEDESTRIAN WARNING SIGN, AND W16-7P SIGN ON POLE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATION IN THE FIELD WITH THE CITY.
(B)	PPB POST & PPB	B	INSTALL PPB POST AND PEDESTRIAN PUSH BUTTON. INSTALL PPB PER CALTRANS DETAIL ES-7A. THE PPB SHALL BE POLARA NAVIGATOR PUSH BUTTON WITH AUDIO MESSAGE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATIONS IN THE FIELD WITH THE CITY.

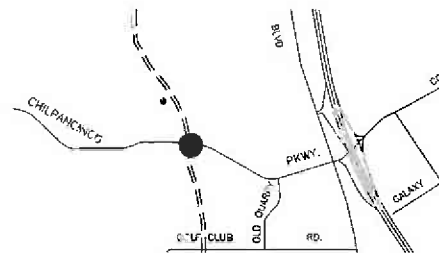
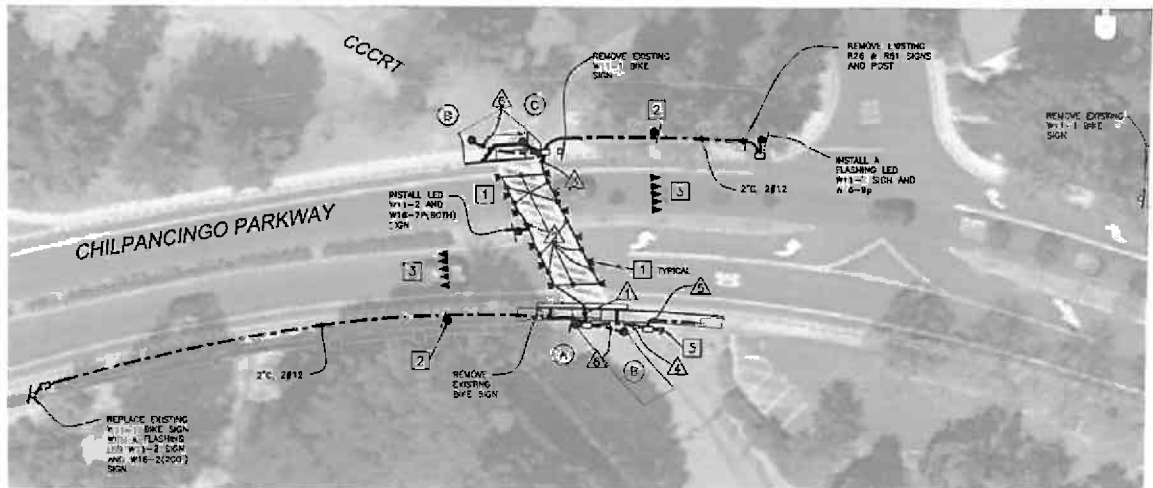
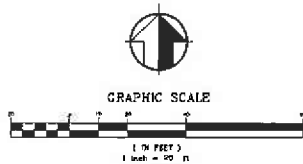
CONDUCTOR SCHEDULE	
No.	
⚠	2" CONDUIT, 4#12, OR PER MANUFACTURER'S REQUIREMENTS.
⚠	STREET CROSSING IN-GROUND CABLE CONNECTION BETWEEN LIGHT FIXTURES & 3#12 PER MANUFACTURER'S REQUIREMENTS.
⚠	2" CONDUIT, 2#12 OR PER MANUFACTURER'S REQUIREMENTS.
⚠	2" CONDUIT, 2#8.
⚠	3" CONDUIT, 3#4 TO BE DETERMINED SERVICE POINT.
1"	

PROJECT NOTES:

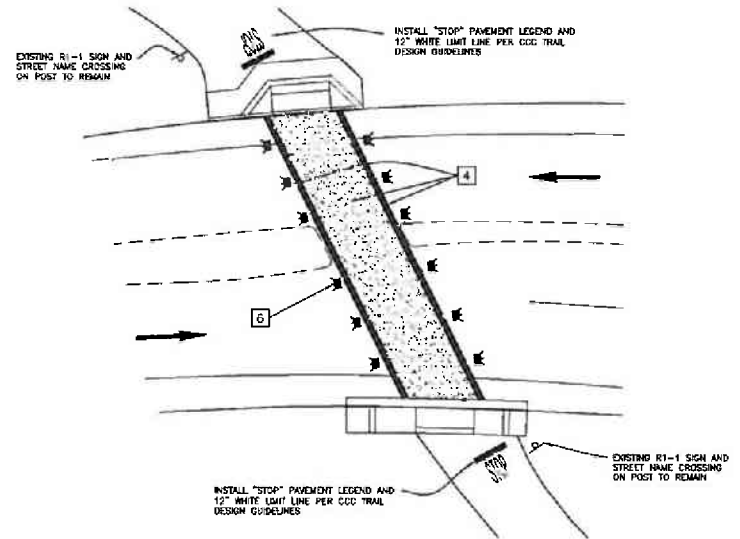
- INSTALL 6-DIRECTIONAL (HARD WIRED) LED IN-PAVEMENT LIGHTS AT LOCATIONS SHOWN. CONTRACTOR SHALL MARK LOCATIONS FOR CITY APPROVAL. LOCATIONS SHALL NOT BE IN THE WHEEL PATH. CONTRACTOR SHALL NOT INSTALL LIGHTS UNTIL LOCATIONS HAVE BEEN APPROVED BY THE CITY.
 - INSTALL R1-SA ON NEW 2" GALVANIZED POLE. LOCATION SHALL BE APPROVED BY CITY PRIOR TO INSTALLATION.
 - INSTALL YIELD LINES, 30" IN ADVANCE OF CROSSWALK, PER FIGURE 3B-14(CA) & 3B-15 OF THE JANUARY 21, 2010 CA-MUTCD.
 - REMOVE EXISTING "LADDER" STRIPING AND INSTALL 'TYRE GRIP' COLOR SURFACE APPLICATION TO CROSSWALK WITH 12" WHITE THERMOPLASTIC STRIPES ON BOTH SIDES.
 - FURNISH AND INSTALL TYPE III-AF SERVICE ENCLOSURE. PG&E SERVICE POINT SHALL BE DETERMINED.
- INSTALL IN-ROAD WARNING LIGHT 12" FROM EDGE OF CROSSWALK STRIPE (TYPICAL)

GENERAL NOTES & LEGENDS:

SEE SHEET TS-2



CONSTRUCTION AREA SIGNS



REVISIONS			
NO.	DESCRIPTION	DATE	BY



City of Pleasant Hill
 DEPARTMENT OF PUBLIC WORKS
 & COMMUNITY DEVELOPMENT
 (925) 671-5264
 100 Gregory Lane
 Pleasant Hill, CA 94523



TJKM Transportation Consultants
 3875 Hopyard Road, Suite 200
 Pleasanton, CA 94588
 Phone: (925) 463-0611 Fax: (925) 463-3690
 email: tjkm@tjkm.com

Designed By: ESB
 Drawn By: TP
 Checked By: JB
 Drawing No.: 05-007 (1)
 Date: 9/12/11

CITY OF PLEASANT HILL
 CITYWIDE TRAIL CROSSING
 ENHANCEMENT PROJECT
 CHILPANCINGO PARKWAY AT CC CRT
 IN-PAVEMENT LIGHTED CROSSWALK

SHEET
 TS-3
 OF
 24

EQUIPMENT SCHEDULE			
No.	Type	QTY	SPECIAL REQUIREMENTS
(A)	1-B POLE	B	INSTALL CONTROLLER CABINET ON POLE PER MANUFACTURER'S SPECIFICATIONS. INSTALL W11-2 LED PEDESTRIAN WARNING SIGN, AND W16-7P SIGN ON POLE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATION IN THE FIELD WITH THE CITY.
(B)	PPB POST & PPB	B	INSTALL PPB POST AND PEDESTRIAN PUSH BUTTON. INSTALL PPB PER CALTRANS DETAIL ES-7A. THE PPB SHALL BE POLARA NAVIGATOR PUSH BUTTON WITH AUDIO MESSAGE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATIONS IN THE FIELD WITH THE CITY.
(C)	1-B POLE	B	INSTALL W11-2 LED PEDESTRIAN WARNING SIGN AND W16-7P SIGN ON POLE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATION IN THE FIELD WITH THE CITY.

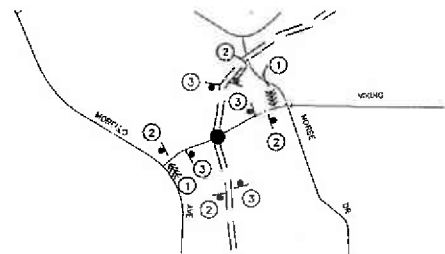
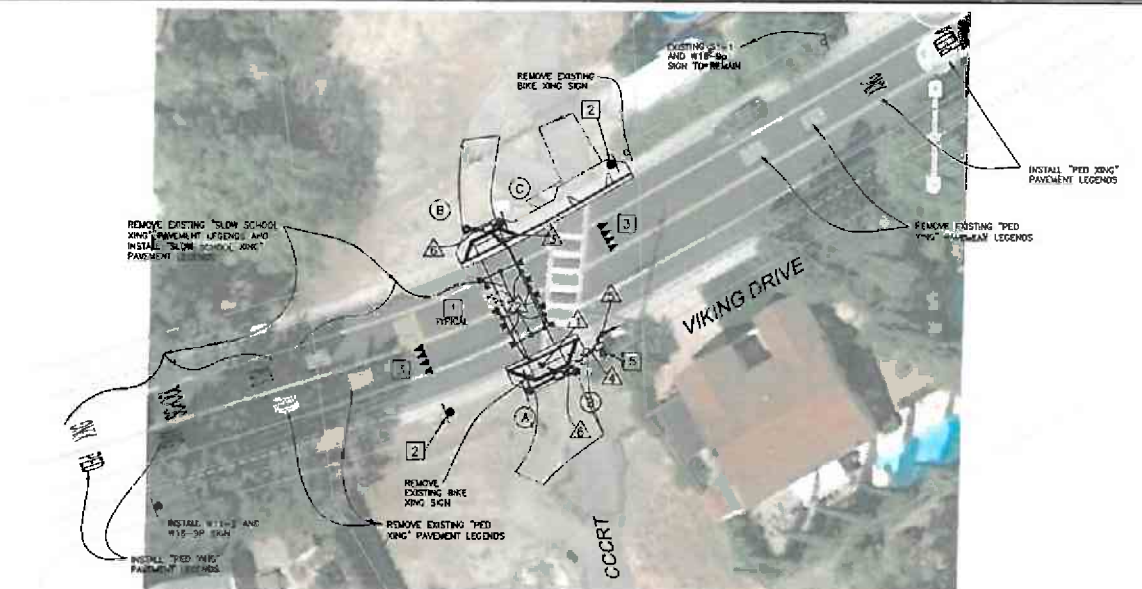
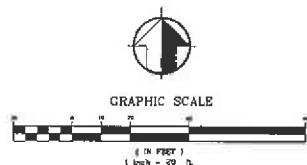
CONDUCTOR SCHEDULE	
No.	
⚠	2" CONDUIT, 4#12, OR PER MANUFACTURER'S REQUIREMENTS.
⚠	STREET CROSSING IN-GROUND CABLE CONNECTION BETWEEN LIGHT FIXTURES & 3#12 PER MANUFACTURER'S REQUIREMENTS.
⚠	2" CONDUIT, 2#12 OR PER MANUFACTURER'S REQUIREMENTS.
⚠	2" CONDUIT, 2#6.
⚠	3" CONDUIT, 3#4 TO BE DETERMINED SERVICE POINT.
⚠	1" CONDUIT, 2#14.

PROJECT NOTES:

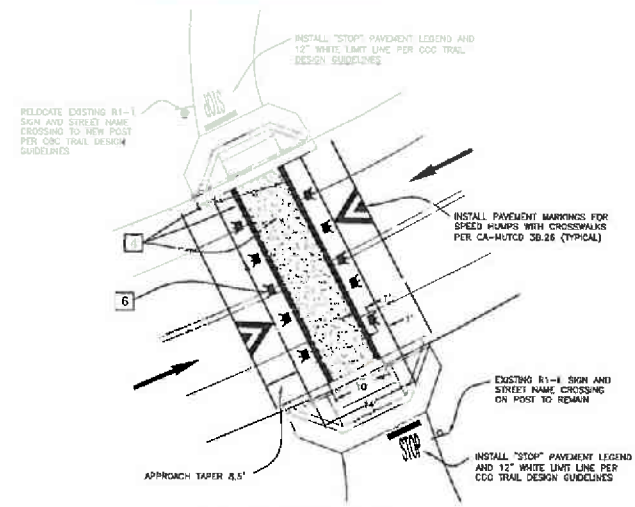
- INSTALL BI-DIRECTIONAL (HARD WIRED) LED IN-PAVEMENT LIGHTS AT LOCATIONS SHOWN. CONTRACTOR SHALL MARK LOCATIONS FOR CITY APPROVAL. LOCATIONS SHALL NOT BE IN THE WHEEL PATH. CONTRACTOR SHALL NOT INSTALL LIGHTS UNTIL LOCATIONS HAVE BEEN APPROVED BY THE CITY.
- INSTALL R1-SA ON NEW 2" GALVANIZED POLE. LOCATION SHALL BE APPROVED BY CITY PRIOR TO INSTALLATION.
- INSTALL YIELD LINES, 30' IN ADVANCE OF CROSSWALK, PER FIGURE 3B-14(CA) & 3B-15 OF THE JANUARY 21, 2010 CA-MUTCD.
- REMOVE EXISTING "ADDER" STRIPING AND INSTALL RAISED CROSSWALK WITH "TYRE GRIP" COLOR SURFACE APPLICATION AND 12" WHITE THERMOPLASTIC STRIPES ON BOTH SIDES.
- FURNISH AND INSTALL TYPE III-AF SERVICE ENCLOSURE. PG&E SERVICE POINT SHALL BE DETERMINED.
- INSTALL IN-ROAD WARNING LIGHT 12" FROM EDGE OF CROSSWALK STRIPE (TYPICAL)

GENERAL NOTES & LEGENDS:

SEE SHEET TS-2



CONSTRUCTION AREA SIGNS



CROSSWALK DETAIL

REVISIONS		
NO.	DESCRIPTION	DATE BY



City of Pleasant Hill
DEPARTMENT OF PUBLIC WORKS
& COMMUNITY DEVELOPMENT
(925) 671-5264
100 Gregory Lane
Pleasant Hill, CA 94523

TJKM Transportation Consultants
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Phone: (925) 463-0511 Fax: (925) 463-3690
email: tjkm@tjkm.com

Designed By:	ECB
Drawn By:	IR
Checked By:	JB
Drawing No.:	55-037 TS
Date:	9/12/11

CITY OF PLEASANT HILL
CITYWIDE TRAIL CROSSING
ENHANCEMENT PROJECT
VIKING DRIVE AT CCRT
IN-PAVEMENT LIGHTED CROSSWALK

SHEET
TS-4
--OF--
24

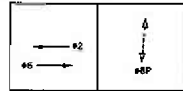
EQUIPMENT SCHEDULE

POLE NO.	TYPE	STANDARD		VEH. SIGNAL M/G.		PED. SIGNAL		PPB	LED LUMINAIRE	SPECIAL REQUIREMENTS**
		SIG. H.X.	LUM. H.X.	MAST ARM	POLE	M/G	ARROW			
(A)	18-4-100	25'	-	MAS	SV-1-T	B	SP-1-T	-	-	INSTALL LED-SNS (ODD TRAIL CROSSING), R3-18 AND E.V.P "A" ON SMA.
(B)	150-TS	-	15'	-	-	-	-	-	(2)E00W	INSTALL ONE VIDEO DETECTION CAMERA FOR GREGORY LANE ON EACH LMA.
(C)	18-4-100	25'	-	MAS	SV-1-T	-	-	-	-	INSTALL LED-SNS (ODD TRAIL CROSSING) #3-18 AND E.V.P "C" ON SMA.
(D)	1-B	-	-	-	-	B	TP-1-T	B	LEFT	
(E)	PPB POST	-	-	-	-	-	-	B	LEFT	

CONDUCTOR SCHEDULE

AWG	CIRCUIT	RUNS					
		1	2	3	4	5	6
NO. 14	#2				3		
	#6		3	3	3		
	PPB	2	2	2	2		
	PPB COMMON	1	1	1	2		1
	SPARES	3	3	3	3		
TOTAL No. 14		7	10	10	15		2
NO. 8	SIGNAL NEUTRAL LUMINAIRE	1	1	1	1		
	TOTAL No. 8	1	1	3	3		2
NO. 6	SIGNALS						2
	TOTAL No. 6						2
NO. 2	POWER						3
	TOTAL CABLES			4	4		
NO. 10	E.V.C		1	1			
	TOTAL CABLES		1	1	2		
CONDUIT SIZE (INCHES)		2"	3"	3"	2-3"	2"	2"
PERCENT CONDUIT FILL		6X	5X	15X	8X	13X	11X

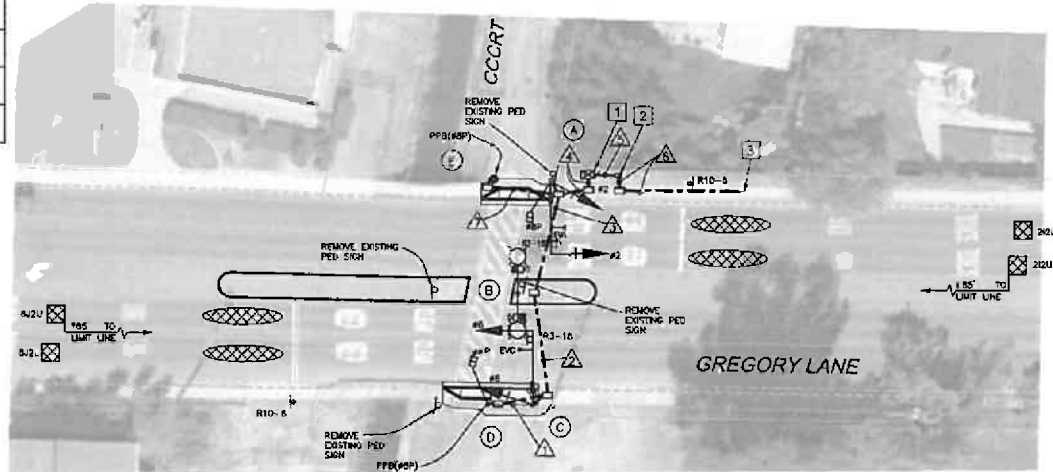
STEADY DEMAND SEQUENCE



PROPOSED PHASE DIAGRAM

EMERGENCY VEHICLE PREEMPT (EVP)

CHANNEL A = #2
CHANNEL C = #6



PROJECT NOTES:

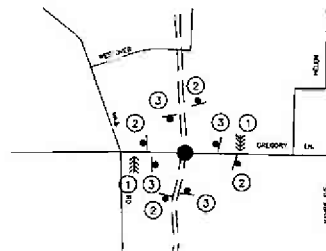
- FURNISH AND INSTALL TYPE 170E CONTROLLER, NEW TYPE 332 CABINET WITH ALL AUXILIARY DEVICES.
- FURNISH AND INSTALL SERVICE ENCLOSURE AND BACK-UP BATTERY SYSTEM PER SPECIFICATIONS.
- TO POINT OF SERVICE, CONTRACTOR SHALL COORDINATE WITH POSE FOR SERVICE CONNECTION, INSTALL CONDUITS, PULL BOXES, AND SERVICE CONNECTION IN ACCORDANCE WITH POSE REQUIREMENTS.
- REMOVE EXISTING "LADDER" STRIPPING AND INSTALL "TIRE GRIP" COLOR SURFACE APPLICATION TO CROSSWALK WITH 12" YELLOW THERMOPLASTIC STRIPES ON BOTH SIDES.

GENERAL NOTES & LEGENDS:

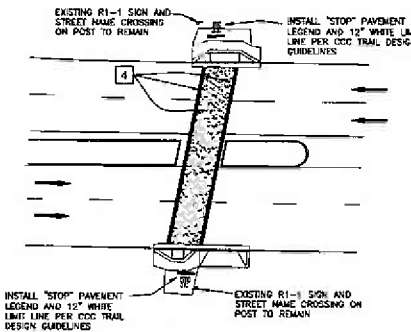
SEE SHEET TS-2



GRAPHIC SCALE



CONSTRUCTION AREA SIGNS



CROSSWALK DETAIL

NO.	REVISIONS	DESCRIPTION	DATE	BY



City of Pleasant Hill
DEPARTMENT OF PUBLIC WORKS
& COMMUNITY DEVELOPMENT
(925) 871-5264
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Pleasant Hill, CA 94523



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email: tjkm@tjkm.com

Designed By: LSH
Drawn By: TR
Checked By: JB
Drawing No.: 85-017 TS
Date: 8/12/11

CITY OF PLEASANT HILL
CITYWIDE TRAIL CROSSING
ENHANCEMENT PROJECT
GREGORY LANE AT CCCR
TRAFFIC SIGNAL INSTALLATION

SHEET
TS-5
OF
24

EQUIPMENT SCHEDULE

No.	Type	QTY	SPECIAL REQUIREMENTS
(A)	1-B POLE	B	INSTALL CONTROLLER CABINET ON POLE PER MANUFACTURER'S SPECIFICATIONS. INSTALL W11-2 LED PEDESTRIAN WARNING SIGN, AND W16-7P SIGN ON POLE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATION IN THE FIELD WITH THE CITY.
(B)	PPB POST & PPB	B	INSTALL PPB POST AND PEDESTRIAN PUSH BUTTON. INSTALL PPB PER CALTRANS MANUAL ES-7A. THE PPB SHALL BE POLARA NAVIGATOR PUSH BUTTON WITH AUDIO MESSAGE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATIONS IN THE FIELD WITH THE CITY.
(C)	1-B POLE	B	INSTALL W11-2 LED PEDESTRIAN WARNING SIGN AND W16-7P SIGN ON POLE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATION IN THE FIELD WITH THE CITY.

CONDUCTOR SCHEDULE

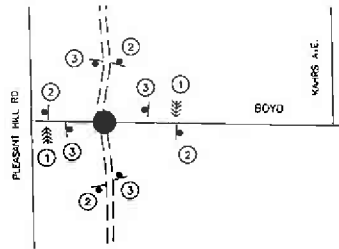
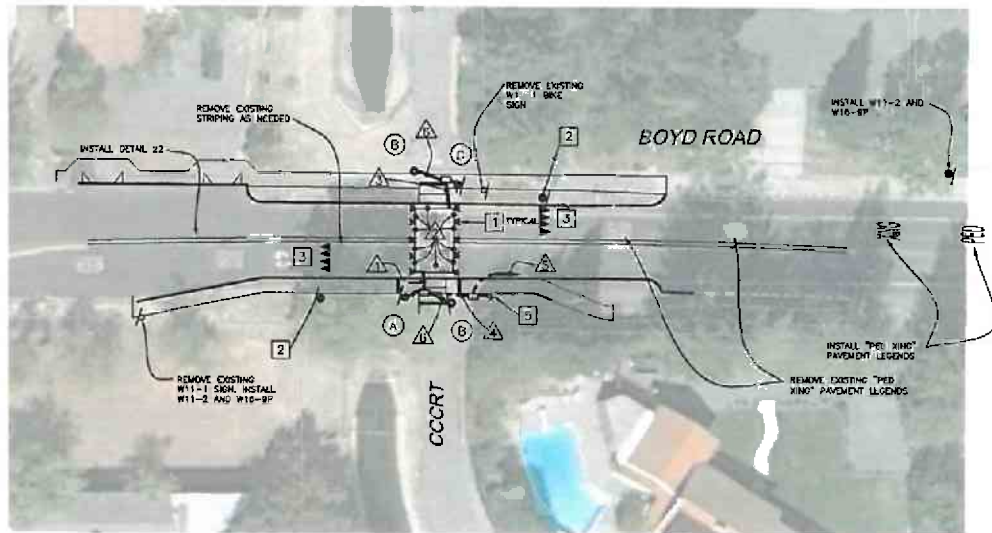
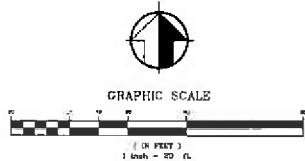
No.	DESCRIPTION
⚠	2" CONDUIT, 4#12, OR PER MANUFACTURER'S REQUIREMENTS.
⚠	STREET CROSSING IN-GROUND CABLE CONNECTION BETWEEN LIGHT FIXTURES & 3#12 PER MANUFACTURER'S REQUIREMENTS.
⚠	2" CONDUIT, 2#12 OR PER MANUFACTURER'S REQUIREMENTS.
⚠	2" CONDUIT, 2#8
⚠	3" CONDUIT, 3#4 TO TO BE DETERMINED SERVICE POINT.
⚠	1" CONDUIT, 2#14.

PROJECT NOTES:

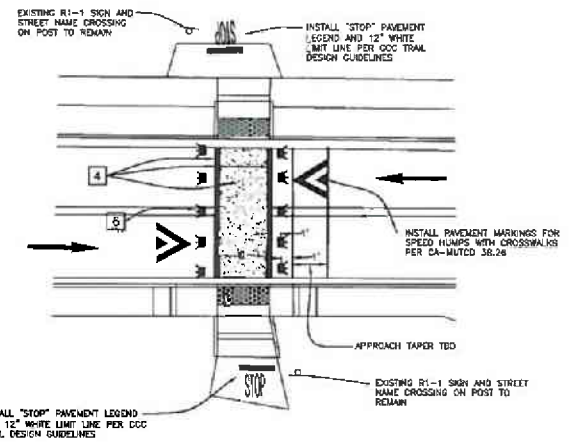
- INSTALL BI-DIRECTIONAL (HARD WIRED) LED IN-PAVEMENT LIGHTS AT LOCATIONS SHOWN. CONTRACTOR SHALL MARK LOCATIONS FOR CITY APPROVAL. LOCATIONS SHALL NOT BE IN THE WHEEL PATH. CONTRACTOR SHALL NOT INSTALL LIGHTS UNTIL LOCATIONS HAVE BEEN APPROVED BY THE CITY.
- INSTALL R1-SA ON NEW 2" GALVANIZED POLE. LOCATION SHALL BE APPROVED BY CITY PRIOR TO INSTALLATION.
- INSTALL YIELD LINES, 30' IN ADVANCE OF CROSSWALK, PER FIGURE 3B-14(CA) & 3B-13 OF THE JANUARY 21, 2010 CA-MUTCD.
- REMOVE EXISTING "LADDER" STRIPING AND INSTALL RAISED CROSSWALK WITH 'TYRE GRIP' COLOR SURFACE APPLICATION AND 12" WHITE THERMOPLASTIC STRIPES ON BOTH SIDES.
- FURNISH AND INSTALL TYPE III-AF SERVICE ENCLOSURE. PG&E SERVICE POINT SHALL BE DETERMINED.
- INSTALL BI-ROAD WARNING LIGHT 12" FROM EDGE OF CROSSWALK STRIPE (TYPICAL)

GENERAL NOTES & LEGENDS:

SEE SHEET TS-2



CONSTRUCTION AREA SIGNS



CROSSWALK DETAIL

NO.	REVISIONS	DATE	BY



City of Pleasant Hill
DEPARTMENT OF PUBLIC WORKS
& COMMUNITY DEVELOPMENT
(925) 671-5284
100 Gregory Lane
Pleasant Hill, CA 94523



Transportation Consultants
3875 Hopyard Road, Suite 200
Pleasanton, CA 94588
Phone: (925) 463-0611 Fax: (925) 463-3690
email: tjkm@tjkm.com

Designed By: ESR
Drawn By: IR
Checked By: JB
Drawing No: 55-037 TS
Date: 9/12/11

CITY OF PLEASANT HILL
CITYWIDE TRAIL CROSSING
ENHANCEMENT PROJECT
BOYD ROAD AT CCRT
IN-PAVEMENT LIGHTED CROSSWALK

SHEET
TS-6
--OF--
24

EQUIPMENT SCHEDULE

No.	Type	#	SPECIAL REQUIREMENTS
(A)	1-B POLE	B	INSTALL CONTROLLER CABINET ON POLE PER MANUFACTURER'S SPECIFICATIONS. INSTALL W11-2 LED PEDESTRIAN WARNING SIGN, AND W16-7P SIGN ON POLE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATIONS IN THE FIELD WITH THE CITY.
(B)	PPB POST & PPB	B	INSTALL PPB POST AND PEDESTRIAN PUSH BUTTON. INSTALL PPB PER CALTRANS DETAIL ES-7A. THE PPB SHALL BE POLARA NAVIGATOR PUSH BUTTON WITH AUDIO MESSAGE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATIONS IN THE FIELD WITH THE CITY.
(C)	1-B POLE	B	INSTALL W11-2 LED PEDESTRIAN WARNING SIGN AND W16-7P SIGN ON POLE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATION IN THE FIELD WITH THE CITY.

CONDUCTOR SCHEDULE

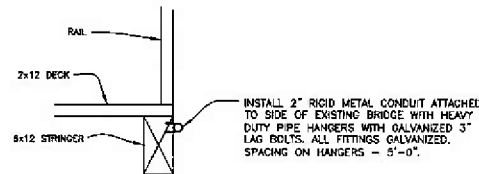
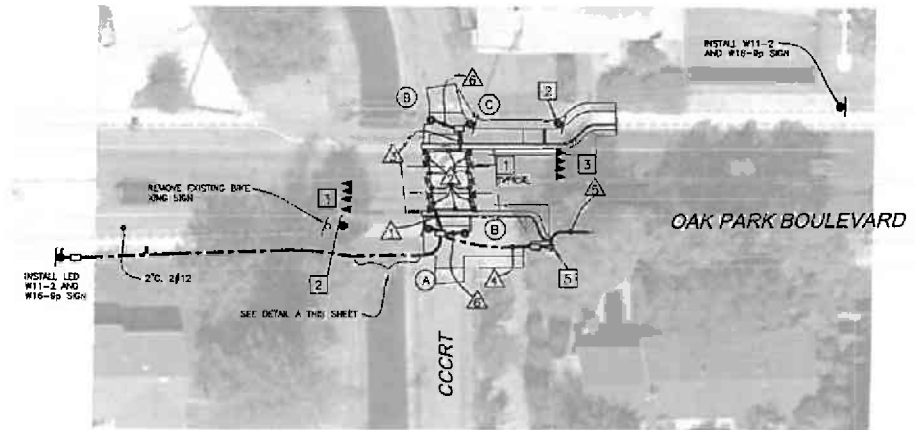
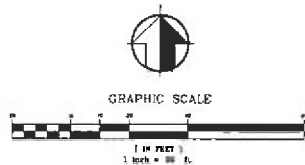
No.	
⚠	2" CONDUIT, 4#12, OR PER MANUFACTURER'S REQUIREMENTS.
⚠	STREET CROSSING IN-GROUND CABLE CONNECTION BETWEEN LIGHT FIXTURES & 3#12 PER MANUFACTURER'S REQUIREMENTS.
⚠	2" CONDUIT, 2#12 OR PER MANUFACTURER'S REQUIREMENTS.
⚠	2" CONDUIT, 2#6.
⚠	3" CONDUIT, 3#4 TO TO BE DETERMINED SERVICE POINT.
⚠	1" CONDUIT, 2#14.

PROJECT NOTES:

- INSTALL BI-DIRECTIONAL (HARD WIRED) LED IN-PAVEMENT LIGHTS AT LOCATIONS SHOWN. CONTRACTOR SHALL MARK LOCATIONS FOR CITY APPROVAL. LOCATIONS SHALL NOT BE IN THE WHEEL PATH. CONTRACTOR SHALL NOT INSTALL LIGHTS UNTIL LOCATIONS HAVE BEEN APPROVED BY THE CITY.
- INSTALL R1-SA ON NEW 2" GALVANIZED POLE. LOCATION SHALL BE APPROVED BY CITY PRIOR TO INSTALLATION.
- INSTALL YIELD LINES, 30' IN ADVANCE OF CROSSWALK, PER FIGURE 3B-14(CA) & 3B-15 OF THE JANUARY 21, 2010 CA-MUTCD.
- REMOVE EXISTING "LADDER" STRIPING AND INSTALL RAISED CROSSWALK WITH "TYRE GRIP" COLOR SURFACE APPLICATION AND 12" WHITE THERMOPLASTIC STRIPES ON BOTH SIDES.
- FURNISH AND INSTALL TYPE II-AF SERVICE ENCLOSURE, PG&E SERVICE POINT SHALL BE DETERMINED.
- INSTALL IN-ROAD WARNING LIGHT 12" FROM EDGE OF CROSSWALK STRIPE (TYPICAL)

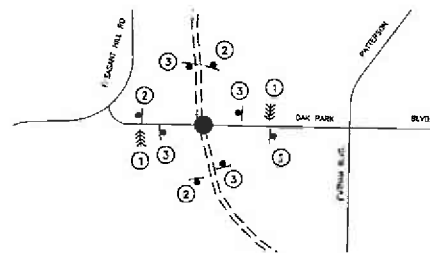
GENERAL NOTES & LEGENDS:

SEE SHEET TS-2

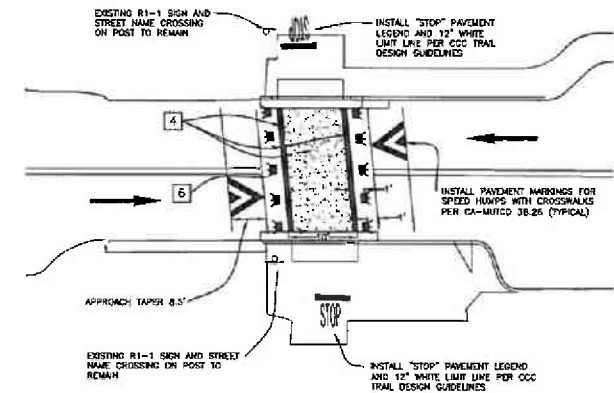


CONDUIT CLAMPED TO EXISTING BRIDGE

DETAIL A



CONSTRUCTION AREA SIGNS



CROSSWALK DETAIL

REVISIONS			
NO.	DESCRIPTION	DATE	BY



City of Pleasant Hill
DEPARTMENT OF PUBLIC WORKS
& COMMUNITY DEVELOPMENT
(925) 671-5284
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Pleasant Hill, CA 94523



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email: tjkm@tjkm.com

Drawn By: ESB
Checked By: TR
Checked By: JB
Drawing No: 65-037-15
Date: 9/17/11

CITY OF PLEASANT HILL
CITYWIDE TRAIL CROSSING
ENHANCEMENT PROJECT
OAK PARK BOULEVARD AT CCCRT
INPAVEMENT LIGHTED CROSSWALK

SHEET
TS-7
--OF--
24

EQUIPMENT SCHEDULE			
No.	Type	#	SPECIAL REQUIREMENTS
(A)	1-B POLE	B	INSTALL CONTROLLER CABINET ON POLE PER MANUFACTURER'S SPECIFICATIONS. INSTALL W11-2 LED PEDESTRIAN WARNING SIGN AND W16-7P SIGN ON POLE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATION IN THE FIELD WITH THE CITY.
(B)	PPB POST & PPB	B	INSTALL PPB POST AND PEDESTRIAN PUSH BUTTON. INSTALL PPB PER CALTRANS DETAIL ES-7A. THE PPB SHALL BE POLARA NAVIGATOR PUSH BUTTON WITH AUDIO MESSAGE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATIONS IN THE FIELD WITH THE CITY.
(C)	1-B POLE	B	INSTALL W11-2 LED PEDESTRIAN WARNING SIGN AND W16-7P SIGN ON POLE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATION IN THE FIELD WITH THE CITY.

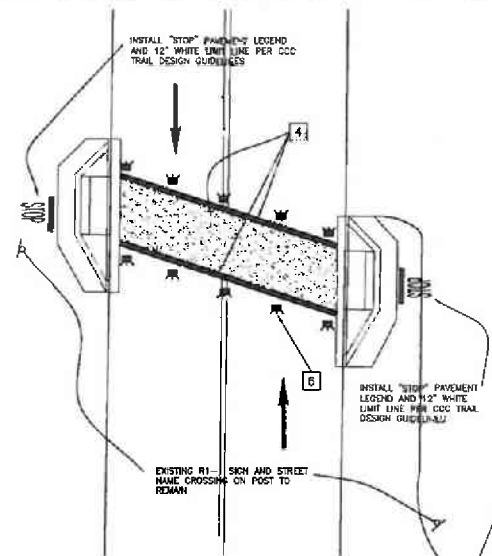
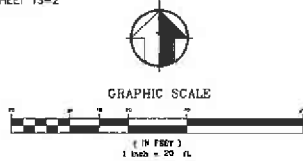
CONDUCTOR SCHEDULE	
No.	
△	2" CONDUIT, 4#12, OR PER MANUFACTURER'S REQUIREMENTS.
△	STREET CROSSING IN-GROUND CABLE CONNECTION BETWEEN LIGHT FIXTURES & 3#12 PER MANUFACTURER'S REQUIREMENTS.
△	2" CONDUIT, 2#12 OR PER MANUFACTURER'S REQUIREMENTS.
△	2" CONDUIT 2#6.
△	3" CONDUIT 3#4 TO TO BE DETERMINED SERVICE POINT.
△	1" CONDUIT, 2#14.

PROJECT NOTES:

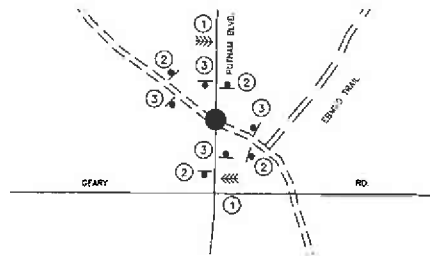
- INSTALL BI-DIRECTIONAL (HARD WIRED) LED IN-PAVEMENT LIGHTS AT LOCATIONS SHOWN. CONTRACTOR SHALL MARK LOCATIONS FOR CITY APPROVAL. LOCATIONS SHALL NOT BE IN THE WHEEL PATH. CONTRACTOR SHALL NOT INSTALL LIGHTS UNTIL LOCATIONS HAVE BEEN APPROVED BY THE CITY.
- INSTALL R1-5A ON NEW 2" GALVANIZED POLE. LOCATION SHALL BE APPROVED BY CITY PRIOR TO INSTALLATION.
- INSTALL YIELD LINES, 30' IN ADVANCE OF CROSSWALK, PER FIGURE 3B-14(C) & 3B-15 OF THE JANUARY 21, 2010 CA-MUTC.
- REMOVE EXISTING "LADDER" STRIPING AND INSTALL "TYRE GRIP" COLOR SURFACE APPLICATION TO CROSSWALK WITH 12" WHITE THERMOPLASTIC STRIPES ON BOTH SIDES.
- FURNISH AND INSTALL TYPE III-AF SERVICE ENCLOSURE. PG&E SERVICE POINT SHALL BE DETERMINED.
- INSTALL IN-ROAD WARNING LIGHT 12" FROM EDGE OF CROSSWALK STRIPE (TYPICAL)
- INSTALL TYPE I PEDESTRIAN BARRICADE [B] WITH R93A & R9-3B(RT) PER ES-7P OF THE MAY 2006 STANDARD PLANS.

GENERAL NOTES & LEGENDS:

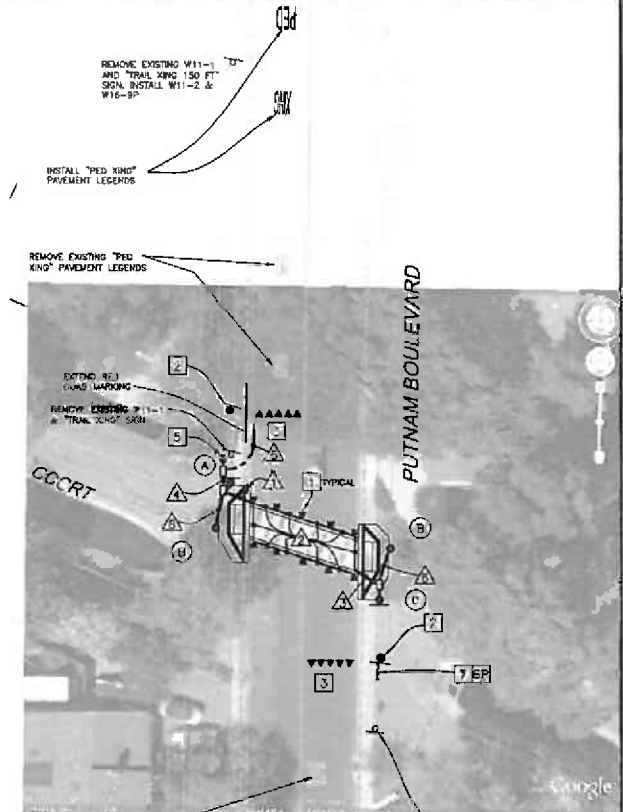
SEE SHEET TS-2



CROSSWALK DETAIL



CONSTRUCTION AREA SIGNS



REMOVE EXISTING W11-1 AND "TRAIL XING" SIGN

INSTALL "PED XING" PAVEMENT LEGENDS

REVISIONS			
NO.	DESCRIPTION	DATE	BY



City of Pleasant Hill
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(925) 671-5264
100 Gregory Lane
Pleasant Hill, CA 94523



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Designed By: ESB
Drawn By: TR
Checked By: JB
Drawing No: 65-C17-17
Date: 9/17/11

CITY OF PLEASANT HILL
CITYWIDE TRAIL CROSSING
ENHANCEMENT PROJECT
PUTNAM BOULEVARD AT CCURT
IN-PAVEMENT LIGHTED CROSSWALK

SHEET
TS-8
--OF--
24

EQUIPMENT SCHEDULE

No.	Type	QTY	SPECIAL REQUIREMENTS
(A)	1-B POLE	B	INSTALL CONTROLLER CABINET ON POLE PER MANUFACTURER'S SPECIFICATIONS. INSTALL 51-1 LED SIGN AND W16-7P SIGN ON POLE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATION IN THE FIELD WITH THE CITY.
(B)	PPB POST & PPE	B	INSTALL PPB POST AND PEDESTRIAN PUSH BUTTON. INSTALL PPB PER CALTRANS DETAIL ES-7A. THE PPB SHALL BE POLARA NAVIGATOR PUSH BUTTON WITH AUDIO MESSAGE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATIONS IN THE FIELD WITH THE CITY.
(C)	1-B POLE	B	INSTALL 51-1 LED AND W16-7P SIGN ON POLE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATION IN THE FIELD WITH THE CITY.

CONDUCTOR SCHEDULE

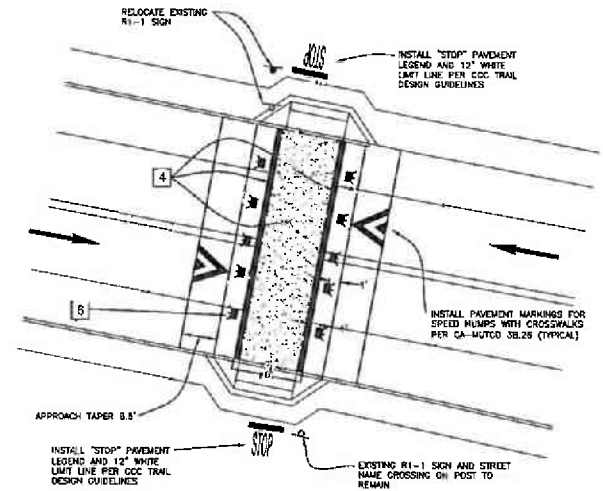
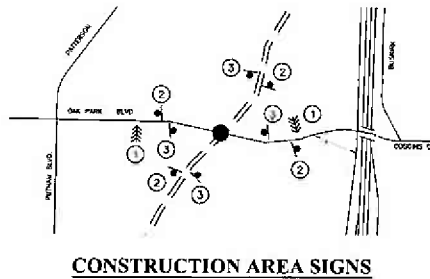
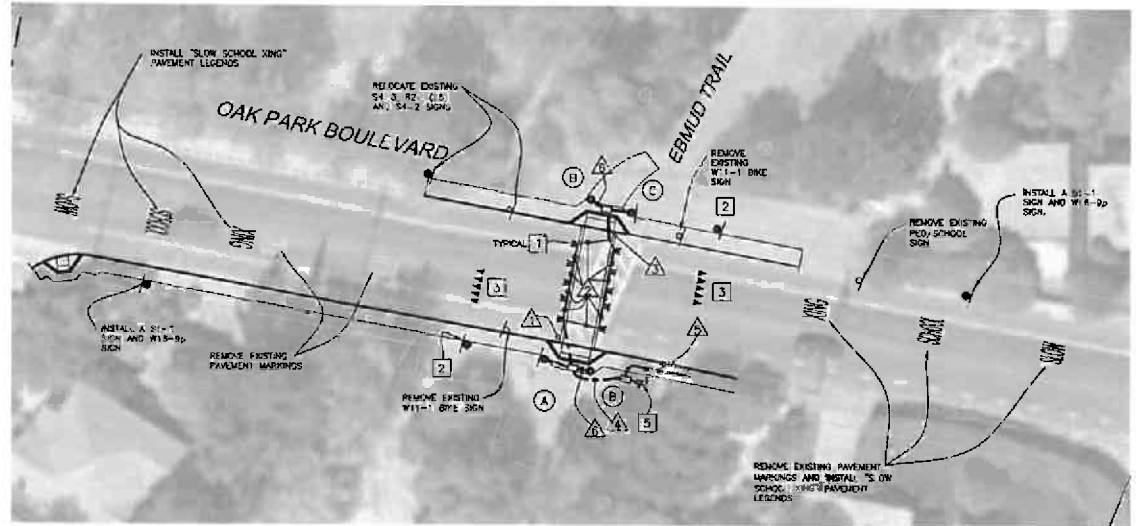
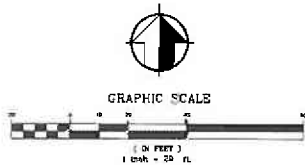
No.	DESCRIPTION
(A)	2" CONDUIT, 4#12, OR PER MANUFACTURER'S REQUIREMENTS.
(B)	STREET CROSSING IN-GROUND CABLE CONNECTION BETWEEN LIGHT FIXTURES & 3#12 PER MANUFACTURER'S REQUIREMENTS.
(C)	2" CONDUIT, 2#12 OR PER MANUFACTURER'S REQUIREMENTS.
(D)	2" CONDUIT, 2#6.
(E)	3" CONDUIT, 3#4 TO BE DETERMINED SERVICE POINT.
(F)	1" CONDUIT, 2#14.

PROJECT NOTES:

- INSTALL BI-DIRECTIONAL (HARD WIRE) LED IN-PAVEMENT LIGHTS AT LOCATIONS SHOWN. CONTRACTOR SHALL MARK LOCATIONS FOR CITY APPROVAL. LOCATIONS SHALL NOT BE IN THE WHEEL PATH. CONTRACTOR SHALL NOT INSTALL LIGHTS UNTIL LOCATIONS HAVE BEEN APPROVED BY THE CITY.
- INSTALL R1-5A ON NEW 2" GALVANIZED POLE. LOCATION SHALL BE APPROVED BY CITY PRIOR TO INSTALLATION.
- INSTALL YIELD LINES, 30' IN ADVANCE OF CROSSWALK, PER FIGURE 3B-14(CA) & 3B-15 OF THE JANUARY 21, 2010 CA MUTCD.
- REMOVE EXISTING "LAUNDER" STRIPING AND INSTALL RAISED CROSSWALK WITH 'TYRE GRIP' COLOR SURFACE APPLICATION AND 12" YELLOW THERMOPLASTIC STRIPES ON BOTH SIDES.
- FURNISH AND INSTALL TYPE III-AF SERVICE ENCLOSURE. PG&E SERVICE POINT SHALL BE DETERMINED.
- INSTALL IN-ROAD WARNING LIGHT 12" FROM EDGE OF CROSSWALK STRIPE (TYPICAL)

GENERAL NOTES & LEGENDS:

SEE SHEET TS-2



NO.	REVISIONS	DATE	BY



City of Pleasant Hill
DEPARTMENT OF PUBLIC WORKS
& COMMUNITY DEVELOPMENT
(925) 671-8284
100 Gregory Lane
Pleasant Hill, CA 94523



Transportation Consultants
3875 Hopyard Road, Suite 200
Pleasanton, CA 94588
Phone: (925) 463-0611 Fax: (925) 463-3690
email: tjkm@tjkm.com

Designed By: ETR
Drawn By: TR
Checked By: JR
Drawing No: 05-037 (1)
Date: 9/12/11

CITY OF PLEASANT HILL
CITYWIDE TRAIL CROSSING
ENHANCEMENT PROJECT
OAK PARK BOULEVARD AT EBMUD TRAIL
IN-PAVEMENT LIGHTED CROSSWALK

SHEET
TS-9
--OF--
24

EQUIPMENT SCHEDULE

No.	Type	QTY	SPECIAL REQUIREMENTS
(A)	1-B POLE	B	INSTALL CONTROLLER CABINET ON POLE PER MANUFACTURER'S SPECIFICATIONS. INSTALL S1-1 LED SIGN, AND W16-7P SIGN ON POLE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATION IN THE FIELD WITH THE CITY.
(B)	PPB POST & PPB	B	INSTALL PPB POST AND PEDESTRIAN PUSH BUTTON. INSTALL PPB PER CALTRANS OF AL 85-7A. THE PPB SHALL BE FOLASA NAVIGATOR PUSH BUTTON WITH AUDIO MESSAGE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATIONS IN THE FIELD WITH THE CITY.
(C)	1-B POLE	B	INSTALL S1-1 LED AND W16-7P SIGN ON POLE. THE CONTRACTOR SHALL VERIFY ALL EQUIPMENT LOCATION IN THE FIELD WITH THE CITY.

CONDUCTOR SCHEDULE

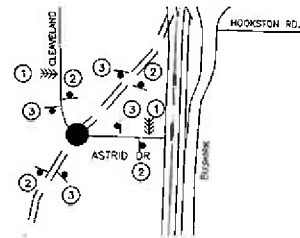
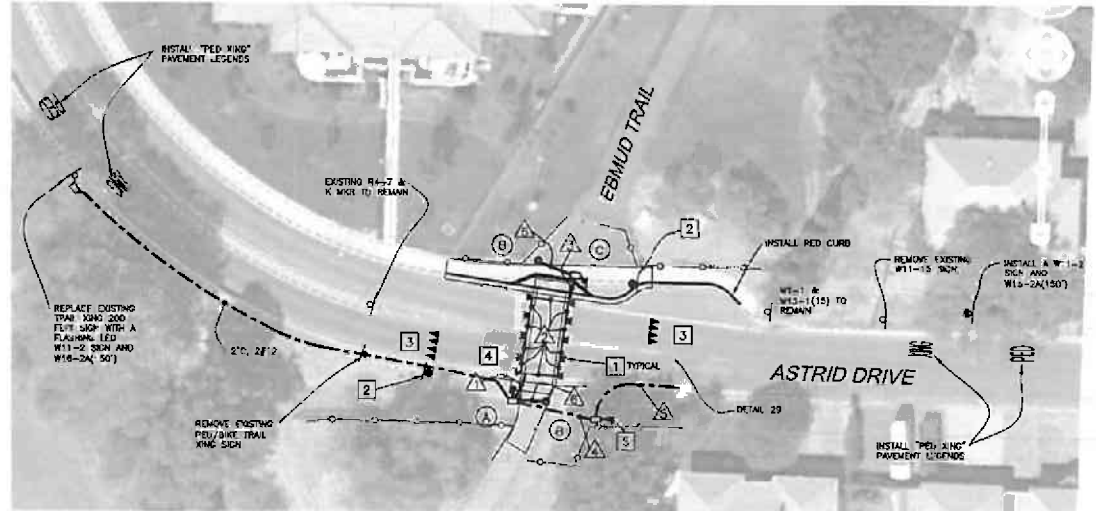
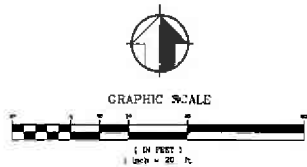
No.	DESCRIPTION
1	2" CONDUIT, 4#12, OR PER MANUFACTURER'S REQUIREMENTS.
2	STREET CROSSING IN-GROUND CABLE CONNECTION BETWEEN LIGHT FIXTURES & 3#12 PER MANUFACTURER'S REQUIREMENTS.
3	2" CONDUIT, 2#12 OR PER MANUFACTURER'S REQUIREMENTS.
4	2" CONDUIT, 2#6.
5	3" CONDUIT, 3#4 TO TO BE DETERMINED SERVICE POINT.
6	1" CONDUIT, 2#14.

PROJECT NOTES:

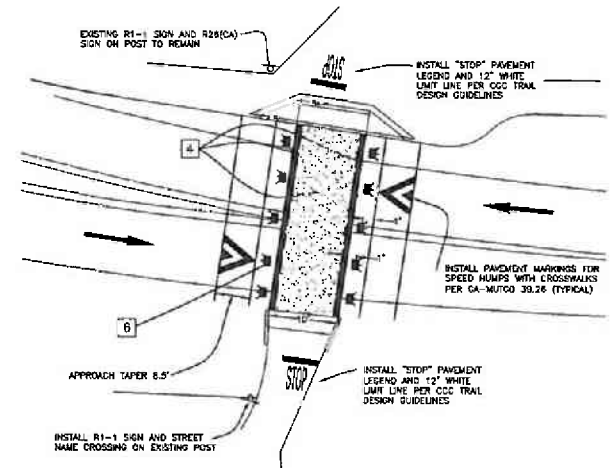
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- REMOVE EXISTING "LADDER" STRIPING AND INSTALL RAISED CROSSWALK WITH 'TYRE GRIP' COLOR SURFACE APPLICATION AND 12" WHITE THERMOPLASTIC STRIPES ON BOTH SIDES.
- FURNISH AND INSTALL TYPE II-AF SERVICE ENCLOSURE. PG&E SERVICE POINT SHALL BE DETERMINED.
- INSTALL IN-ROAD WARNING LIGHT 12" FROM EDGE OF CROSSWALK STRIPE (TYPICAL)

GENERAL NOTES & LEGENDS:

SEE SHEET TS-2



CONSTRUCTION AREA SIGNS



CROSSWALK DETAIL

NO.	REVISIONS	DATE	BY



City of Pleasant Hill
DEPARTMENT OF PUBLIC WORKS
& COMMUNITY DEVELOPMENT
(925) 871-5264
100 Gregory Lane
Pleasant Hill, CA 94523



TJKM Transportation Consultants
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Pleasanton, CA 94566
Phone: (925) 463-0811 Fax: (925) 463-3690
email: tjkm@tkm.com

Designed By: CCB
Drawn By: TR
Checked By: JB
Drawing No: 07-01715
Date: 9/15/11

CITY OF PLEASANT HILL
CITYWIDE TRAIL CROSSING
ENHANCEMENT PROJECT
ASTRID DRIVE AT EBMUD TRAIL
IN-PAVEMENT LIGHTED CROSSWALK

SHEET
TS-10
OF
24